

5 The Plan – Land Regulations

5.1 Introduction

This Plan establishes land use and design regulations that enable East Gateway to become a more complete community — one with conveniently located housing, employment, shopping, services, and recreation served by multi-modal transportation. The residential development and employment allowed with Sector Development Plan zoning will place existing and future residents and employees within a convenient walk, bike ride or drive to shopping, services, Rapid Ride and other transit service.

This chapter establishes four SU-2 zoning districts. Unlike SU-1 zones that are each tailored to special uses on individual public and private properties, SU-2 zones are created to help achieve the specific goals of a sector development plan and apply to many properties within sector development plan boundaries.

Three of the SU-2 zoning districts allow a wide variety of uses for properties abutting or close to Central Avenue or Eubank Boulevard. A fourth SU-2 zone allows a more limited list of uses for properties that, before the adoption of this Plan, were zoned for community commercial uses inside residential neighborhoods near Central Avenue.

Properties with residential, light manufacturing, heavy commercial, or SU-1 zoning established before the adoption of this Plan are not rezoned by this Plan. SU-1 zoned properties may continue to develop with the uses assigned under the specific SU-1 designation for that property or may develop with the SU-2 zone uses described for the SU-2 zone also assigned to that property. (See Existing Zoning Map in Section 4.4 and East Gateway Zoning Maps on pages 5-12 through 5-22.)

Besides allowing a wide variety of uses, the Plan establishes building form, lot layout and site design to help make East Gateway work better for people walking, cycling, taking transit or driving. This approach enables predictable development outcomes for the developer, property owner, City review authorities and the community.

The Plan allows a wide variety of building types for new development and redevelopment on properties in the four SU-2 zoning districts. Each building type is associated with site development standards.

The Plan establishes General Design Regulations to address site, building, and public right-of-way aesthetics and function. Most general design regulations apply to the SU-2 zoned properties and properties abutting area arterial streets. A few apply to other non-residential properties.

This Plan also rezones City-owned Major Public Open Space parcels to SU-1 for Major Public Open Space to ensure their future use for these purposes. (See East Gateway Plan Zoning Map on page 5-6.)

5.2 Navigating the Zoning Regulations

1. Examine the following maps to determine whether your property is within the General Design Regulation boundary. If it is, then go to Section 5.3 of this chapter. Subsection 5.3.1 will help you determine if your development proposal triggers compliance with general design regulations in Section 5.6. Subsections 5.3.2 and 5.3.3 describe the approval process and approval process exceptions.
2. Examine the following maps again to determine whether your property is designated SU-2/EG-CAC (Community Activity Zone), SU-2/EG-NAC (Neighborhood Activity Zone), SU-2/EG-C (Corridor Zone), or SU-2/EG-C-2 (Community Commercial Zone). If your property is located within one of these zoning districts and your development proposal triggers compliance with the uses established in one of these zoning districts, then go to the pertinent zone in Section 5.4 to determine the allowed uses. If your property does not meet the tests established by the City of Albuquerque Comprehensive City Zoning Code for legal nonconforming uses, then the uses listed under the pertinent zoning district apply.
3. If your property is zoned SU-2/EG-CAC, SU-2/EG-NAC, SU-2/EG-C, or SU-2/EG-C-2 and your development proposal triggers compliance with building type and lot indicated in Subsection 5.3.1, then go to Section 5.5 to choose the appropriate building type, building frontage and lot design for your development.
4. If your property is zoned SU-2/EG-CAC, SU-2/EG-NAC, SU-2/EG-C, or SU-2/EG-C-2 or retains the zone established before Plan adoption, but is within the General Design Regulation boundary, then go to Section 5.6 to determine the design regulations for site, building and/or public right-of-way that apply to your property.

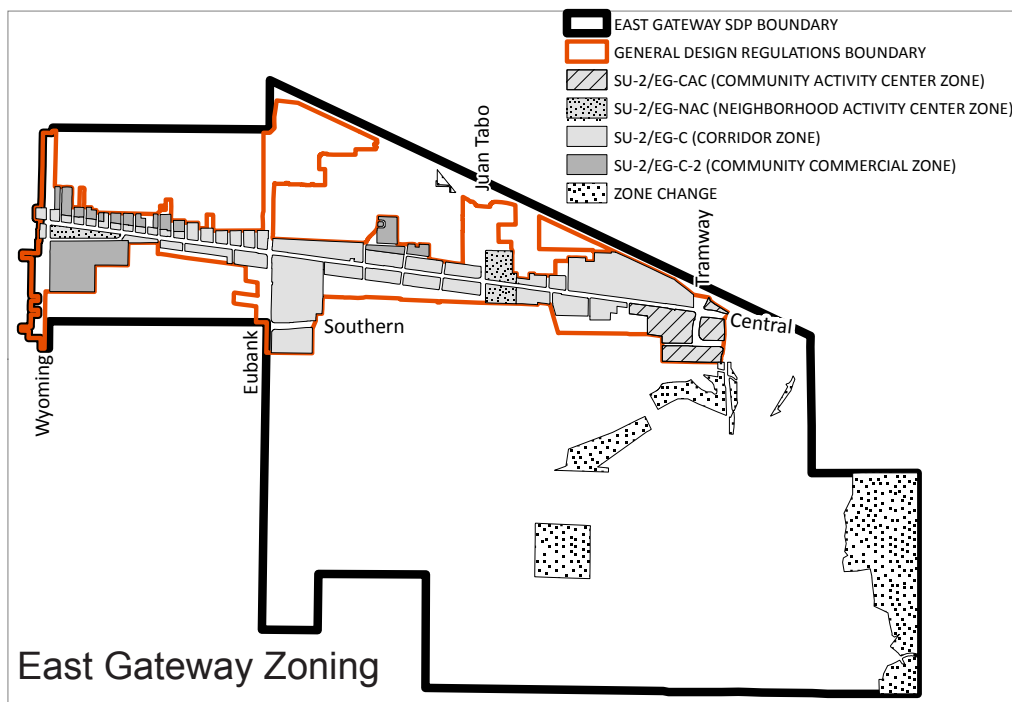
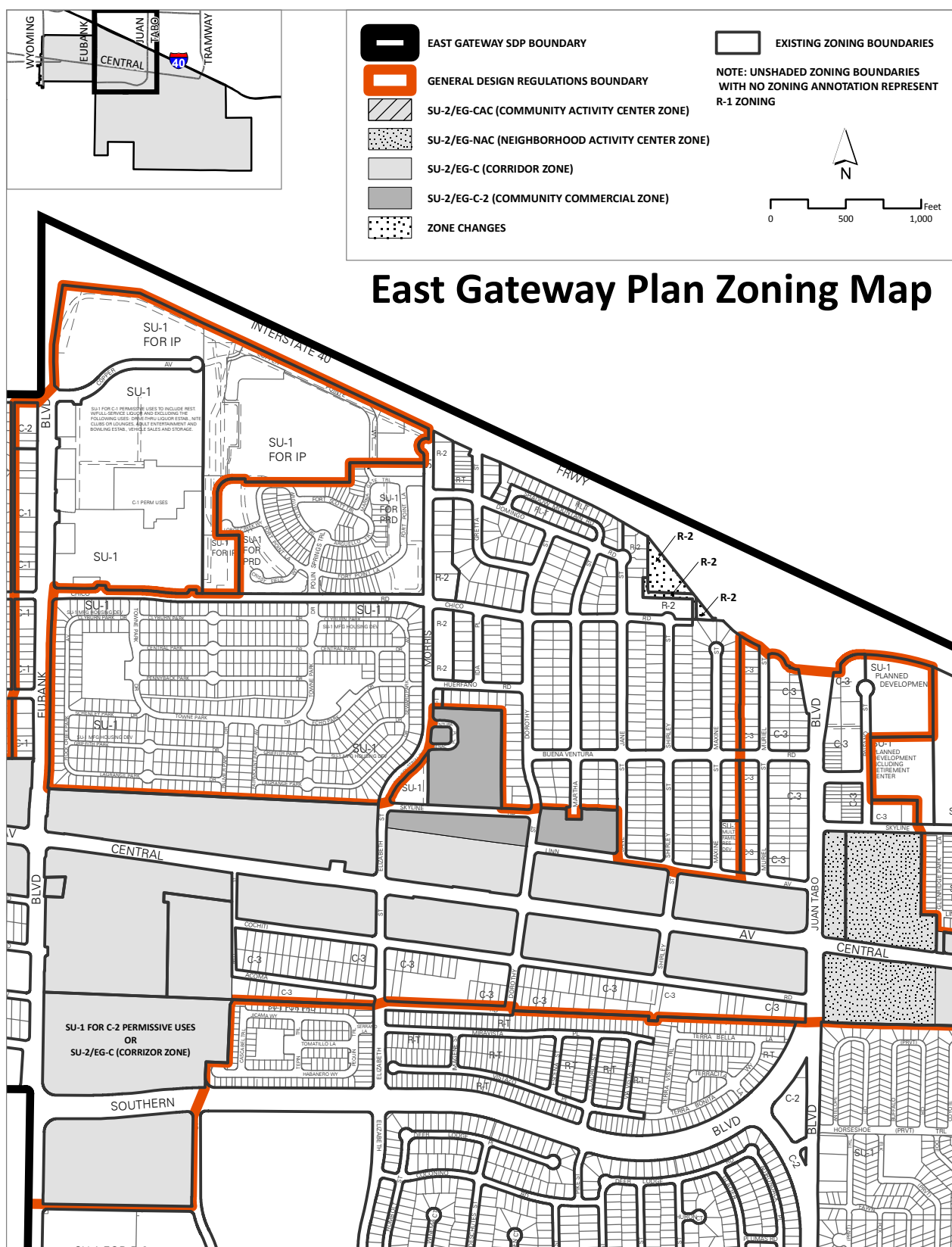


Figure 5.a: East Gateway Plan Zoning Map



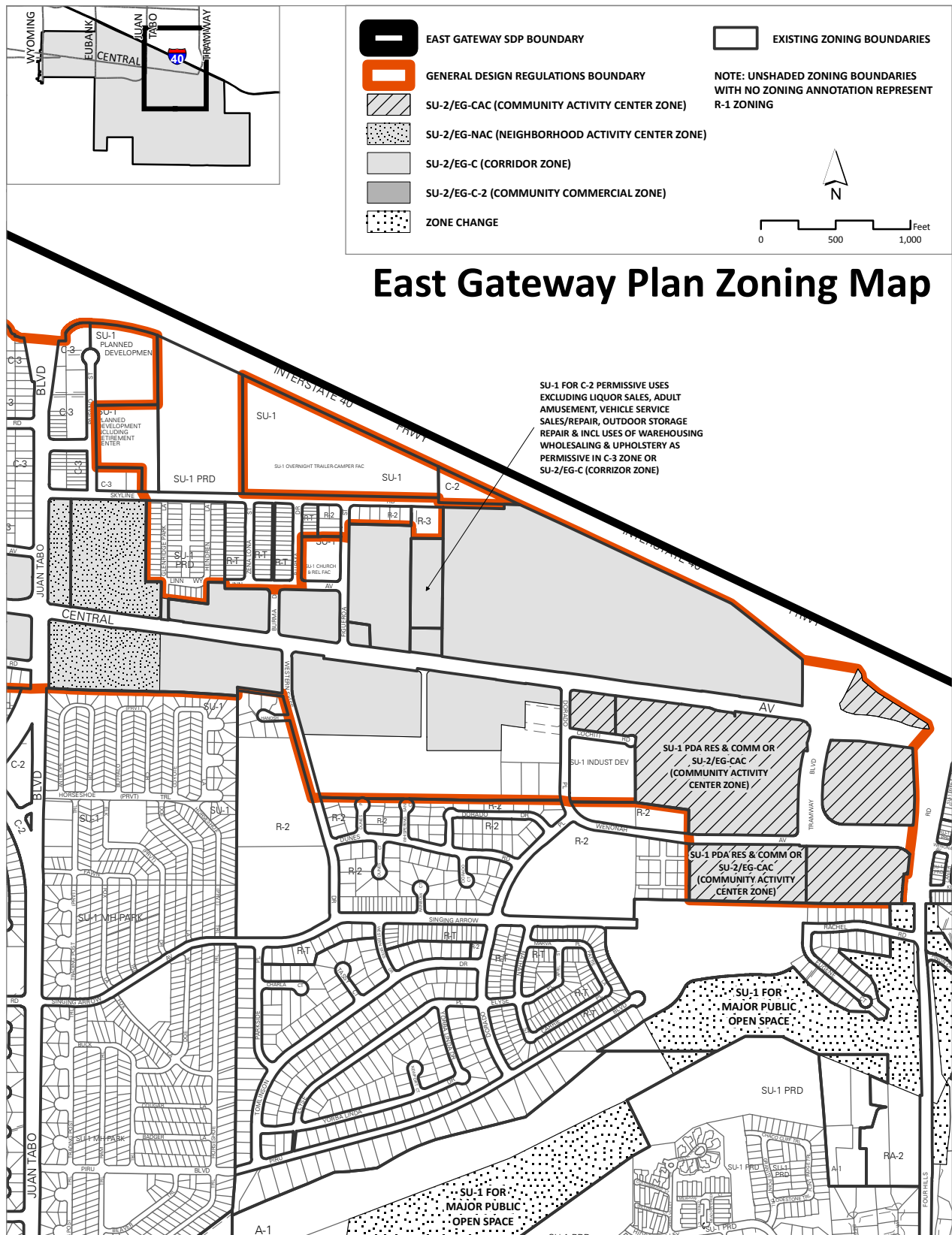


Figure 5.d: East Gateway Plan Zoning Map
(Juan Tabo Boulevard - Four Hills Road)

5.3 Development Approval Process

This Section includes the conditions that trigger development compliance with East Gateway regulations, the development review process, and the approval process for requesting development modifications to Sector Development Plan design regulations.

5.3.1 Development Compliance Triggers

- A. The following development requests within East Gateway SU-2 Zones or General Design Regulation boundaries shall comply with pertinent East Gateway Sector Development Plan regulations in Sections 5.5 and 5.6 of this chapter:
 - 1. New development
 - 2. Building additions of 15% or more of existing building square footage
 - 3. Buildings replacing existing buildings including buildings that are destroyed by flood, fire, or natural catastrophe
 - 4. Amendments to SU-1 site plans that include additions of 15% or more of existing building square footage
- B. The following activities are exempt from compliance with the regulations of Sections 5.5 and 5.6 of this Plan, but shall adhere to pertinent regulations of the City of Albuquerque Comprehensive City Zoning Code:
 - 1. Repairs, remodeling and maintenance of existing structures and/or buildings
 - 2. Facade improvements to existing buildings
- C. East Gateway Sector Development Plan SU-2 Zone designation may cause some permissive and conditional uses existing at the time of Plan adoption to become nonconforming uses. These uses shall be considered legal nonconforming uses as provided in the City of Albuquerque Comprehensive City Zoning Code.

5.3.2 Development Approval Process

Development occurring within the East Gateway General Design Regulations boundary requires approval as outlined below.

Approval Body	Development Type	Notification
Building Permit Staff	All three of the following conditions are necessary: <ul style="list-style-type: none">• Site less than 5 acres excepting site with SU-1 zoning• Proposed Use is a Permissive Use• Development complies with Building and Lot Standards in Section 5.5 of this chapter and General Design Regulations in Section 5.6 of this chapter	No Public Notification is required
Administrative Review and Approval by Planning Director or Designee preceded by Pre-Application Meeting with the Pre-Application Review Team (PRT)	Site 5 acres or greater including site with SU-1 Zoning that does not meet the thresholds for EPC review in 14-16-2-22 of the City Zoning Code, but excepting a Large Retail Facility as defined in the City Zoning Code	No Public Notification is required
ZHE	Conditional Use	Public Notification is required
EPC	One or more of the following conditions: <ul style="list-style-type: none">• Site with SU-1 zoning that meets the thresholds for EPC review in 14-16-2-22 of the City Zoning Code• A Large Retail Facility as defined in the City Zoning Code• Not a Permissive Use• Development that deviates 10% or more from dimensional Building and Lot Standards in Section 5.5 of this chapter• Development does not comply with General Design Regulations in Section 5.6 of this chapter	Public Notification is required

5.3.3 Requests to Deviate from Sector Development Plan Building and Lot Standards in Section 5.5 of this Chapter and General Design Regulations in Section 5.6 of this Chapter

The building and lot standards in Section 5.5 of this chapter and General Design Regulation in Section 5.6 of this chapter are specific and prescriptive to provide certainty for applicants, neighborhoods, and City development reviewers. It is not the intent of these regulations to limit design creativity or ignore unusual site conditions. As such, the following two levels of modifications to the zoning regulations are allowed:

- A. Minor: The Planning Director or his/her designee may approve deviations of less than 10% from any dimensional standards.
- B. Major: The EPC shall review any deviation of 10% or more from any dimensional standard to determine if the request undermines the intent of the zoning requirement.

5.4 The SU-2 Zoning Districts

Chapter Subsection 5.4 contains four SU-2 zoning districts for community activity centers, neighborhood activity centers, major street corridor segments between activity centers, and formerly C-2 zoned land within residential neighborhoods. The zoning districts establish permissive and conditional uses for properties that do not meet the requirements to be legal nonconforming uses. Building, lot, and building facade types for new construction in these zones are located in Section 5.5 of this Plan chapter. General regulations for site, building and public right-of-way are located in Section 5.6 of this chapter.

The four SU-2 zones established by this Plan are 1) East Gateway Community Activity Center Zone (SU-2/EG-CAC), 2) East Gateway Neighborhood Activity Center Zone (SU-2/EG-NAC), 3) East Gateway Corridor Zone (SU-2/EG-C), and 4) East Gateway Community Commercial Zone (SU-2/EG-C-2).

Properties already zoned SU-1 may continue to develop with the uses assigned under the specific SU-1 designation for that property or may develop with the SU-2 zone uses described for the SU-2 zone also assigned to that property. All SU-1 properties within the general regulations boundary shall comply with regulations in Section 5.5 and 5.6 of this chapter.

5.4.1 SU-2 East Gateway Community Activity Center Zone (SU-2/EG-CAC)

ZONE INTENT

This zoning district enables future mixed-use redevelopment of an area at the Central Avenue/Tramway Boulevard intersection already designated a Community Activity Center by the Albuquerque/Bernalillo County Comprehensive Plan. (See maps of these properties following permissive uses of this zone.)

Community Activity Centers are usually between 30 and 60 acres. They can be larger. Their buildings, streets, and walkways are pedestrian-oriented in design and are intended to provide a mixture of some or all of the following uses: stores, offices, medical services, day care, entertainment, higher-density residences, and/or institutions like schools, libraries, and other public services.

Community Activity Centers are generally spaced to serve people living within a radius of up to three miles. Community Activity Centers are prime locations for transit hubs because they serve concentrations of residents, employees, shoppers, and people accessing entertainment.

PERMISSIVE USES

1. Uses listed as permissive in 14-16-2-17 C -2 Community Commercial Zone, with the following exceptions:
 - a. Antennas are not allowed.
 - b. Drive-up and drive-in facilities are not allowed.
 - c. Gasoline, oil, liquefied petroleum gas, or other vehicle fuel sales are not allowed.
 - d. Parking lots (parking as an individual use rather than required on-site parking) are not allowed.
 - e. Signs: On-premise signs are permitted as regulated in the O-1 Zone 14-16-2-15 and General Sign Regulations 14-16-3-5 of the City Zoning Code except that neon signs are allowed. Off-premise signs are not allowed. Live/work signs may be no more than 8 square feet in area and shall be located on the building wall no higher than the first floor.
 - f. Taxidermy services are not allowed.
 - g. Vehicle sales, rental, service repair, or storage are not allowed.
 - h. Vehicle washing is not allowed, unless located in a parking structure.
 - i. Wireless Telecommunications Facility is allowed only if it is attached to a building and does not exceed building height.
2. Apartments as defined in the City Zoning Code and controlled in the East Gateway Building Types (Section 5.5 of this Plan) and General Design Regulations (Section 5.6 of this Plan).
3. Live/Work Space in the form of an individual unit, building, or multiple buildings on one premise that are used for both residence and business with residents responsible for the business activity conducted on location. No separation between residential and business activities is required. Live/Work Spaces shall include residences and may include one or more of the following uses:
 - a. Office
 - b. Retail sales of the following goods, plus incidental service and repair and

incidental retailing of related goods:

- Arts and crafts objects and supplies
- Books, magazines, newspapers, stationery, excepting adult bookstore
- Cosmetics, notions, hobby supplies
- Flowers and plants
- Jewelry
- Clothing

c. Services:

- Barber, beauty
- Day care center
- Instruction in music, dance, fine arts or crafts
- Photography, except adult photo studio
- Tailoring, dressmaking

d. Arts and crafts production and manufacturing businesses that comply with federal, state, and local environmental regulations and use 2,000 square feet or less of space.

e. Residential floor space that is a minimum of 300 square feet per unit for the first residential occupant and 150 square feet per unit for each additional residential occupant.

f. Units and buildings shall comply with 14-2-1 Fire Code and currently adopted Building Code for using kilns, centrifugal casting, spray-painting booths, and other art manufacturing processes.

g. Odorous, noisy and dangerous processes that are incompatible with living on site are not allowed.

4. Transit stops and transit facilities

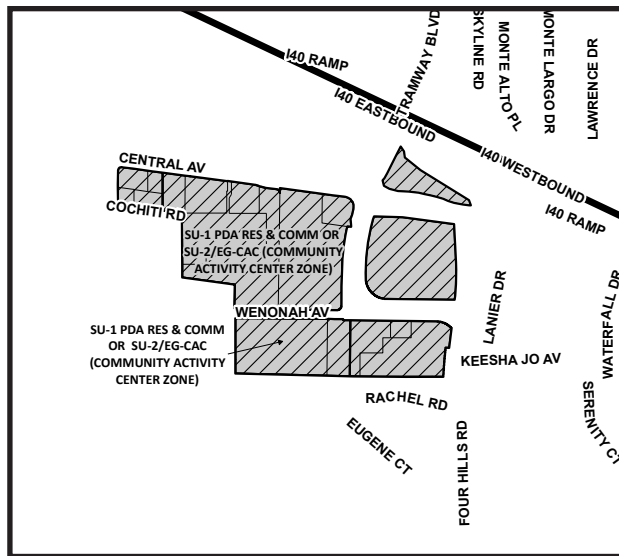


Figure 5.f: East Gateway Sector Development Plan
SU-2/EG-CAC (Community Activity Center) Zone

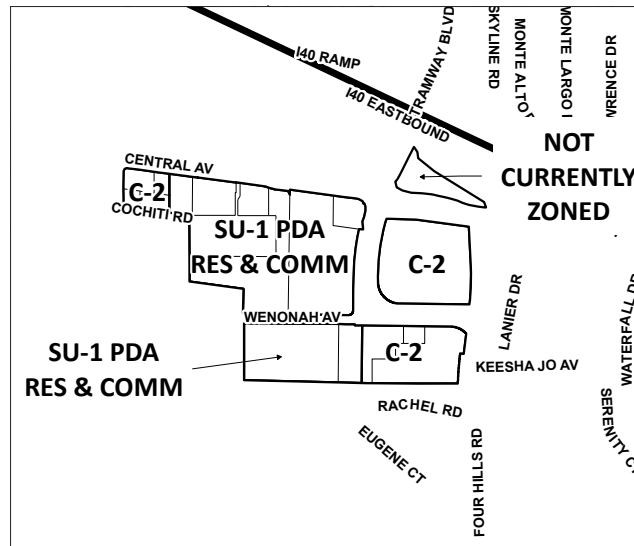


Figure 5.g: Zoning Predating the
East Gateway Sector Development Plan

5.4.2 SU-2 East Gateway Neighborhood Activity Center Zone (SU-2/EG-NAC)

ZONE INTENT

This zoning district enables the future development of two areas this Plan designates as Neighborhood Activity Centers: one at the Central Avenue/Juan Tabo Boulevard intersection and another at the Central Avenue/Wyoming Boulevard intersection. (See maps of these properties following the description of permissive uses of this zone.)

Neighborhood Activity Centers are usually between 5 and 15 acres. Their buildings, streets, and walkways are pedestrian-oriented in design. They are intended to provide mixtures of services and residential building types not found within single-family residential neighborhood interiors.

Buildings in Neighborhood Activity Centers are smaller in scale than Community Activity Centers. They should be easily accessible destinations for nearby residents and others, making it possible for nearby residents and transit riders to access local services within a one-quarter to half-mile walk.

PERMISSIVE USES

1. Uses listed as permissive in 14-16-2-16 C-1 Neighborhood Commercial Zone, with the following exceptions:
 - a. Antennas are not allowed.
 - b. Drive-up and drive-in facilities are not allowed.
 - c. Residential Uses permissive in the R-3 zone are allowed, but are not required to comply with the C-1 zone minimum acreage requirement.
 - d. Auto parts and supplies are not allowed.
 - e. Gasoline, oil, liquefied petroleum gas, or other vehicle fuel sales are not allowed.
 - f. Signs: On-premise signs are permitted as regulated in the O-1 Zone 14-16-2-15 and General Sign Regulations 14-16-3-5 of the City Zoning Code except that neon signs are allowed. Off-premise signs are not allowed. Live/work signs may be no more than 8 square feet in area and shall be located on the building wall no higher than the first floor.
 - g. Taxidermy is not allowed.
 - h. Wireless Telecommunications Facility is allowed only if it is attached to a building, does not exceed building height and is concealed.
2. Townhouses and Apartments as defined in the City Zoning Code and controlled in the East Gateway Building Types (Section 5.5 of this Plan) and General Design Regulations (Section 5.6 of this Plan).
3. Live/Work Space in the form of an individual unit, building, or multiple buildings on one premise that are used for both residence and business with residents responsible for the business activity conducted on location. No separation between residential and business activities is required. Live/Work Spaces shall include residences and may include one or more of the following uses:
 - a. Office
 - b. Retail sales of the following goods, plus incidental service and repair and incidental retailing of related goods:
 - Arts and crafts objects and supplies

- Books, magazines, newspapers, stationery, excepting adult bookstore
 - Cosmetics, notions, hobby supplies
 - Flowers and plants
 - Jewelry
 - Clothing
 - c. Services:
 - Barber, beauty
 - Day care center
 - Instruction in music, dance, fine arts or crafts
 - Photography, except adult photo studio
 - Tailoring, dressmaking
 - d. Arts and crafts production and manufacturing businesses that comply with federal, state, and local environmental regulations and use 2,000 square feet or less space.
 - e. Residential floor space that is a minimum of 300 square feet per unit for the first residential occupant and 150 square feet per unit for each additional residential occupant.
 - f. Units and buildings shall comply with 14-2-1 Fire Code and currently adopted Building Code for using kilns, centrifugal casting, spray-painting booths, and other art manufacturing processes.
 - g. Odorous, noisy and dangerous processes that are incompatible with living on site are not allowed.
4. Transit stops and transit facilities

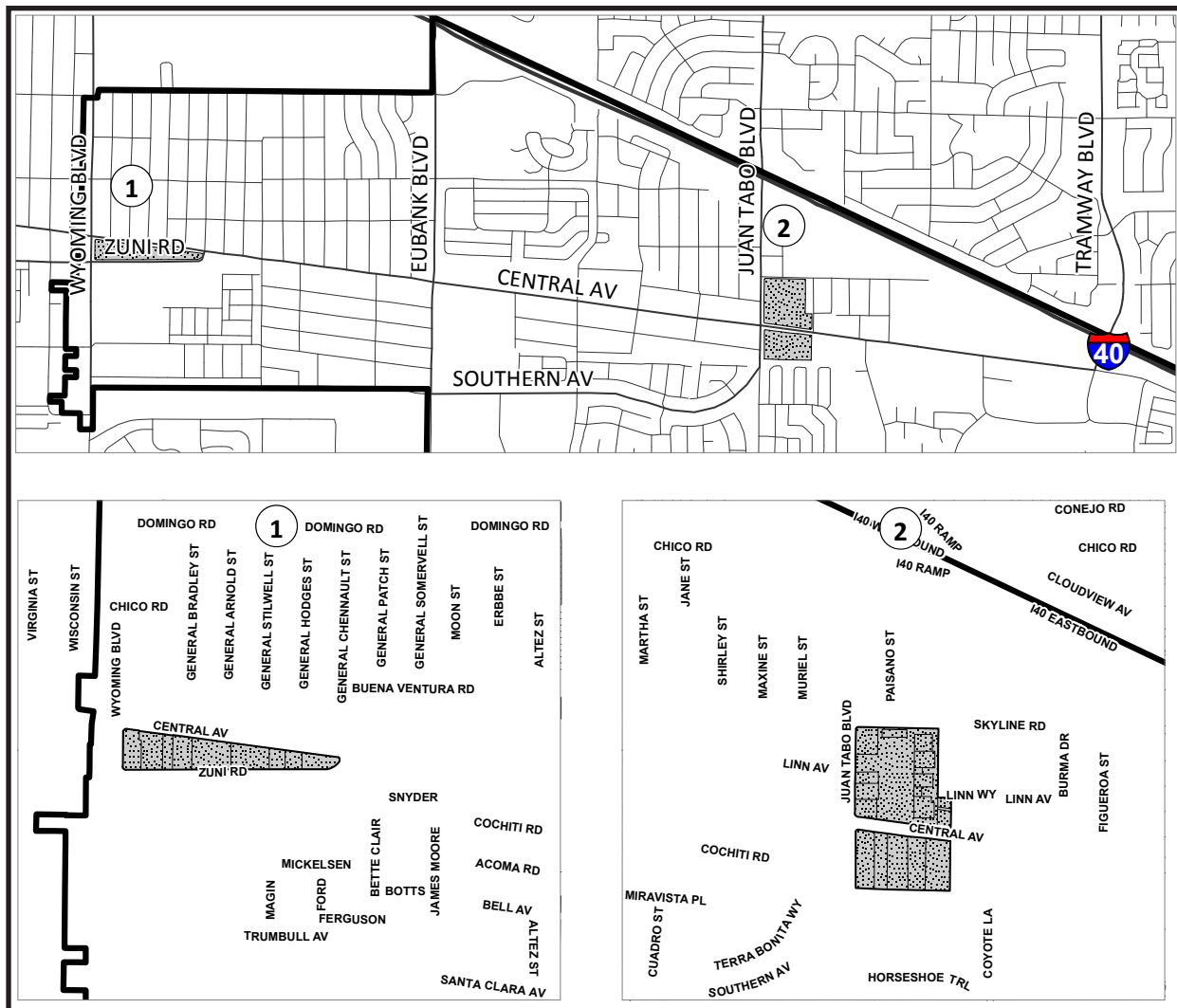


Figure 5.h: East Gateway Sector Development Plan SU-2/EG-NAC (Neighborhood Activity Center) Zone

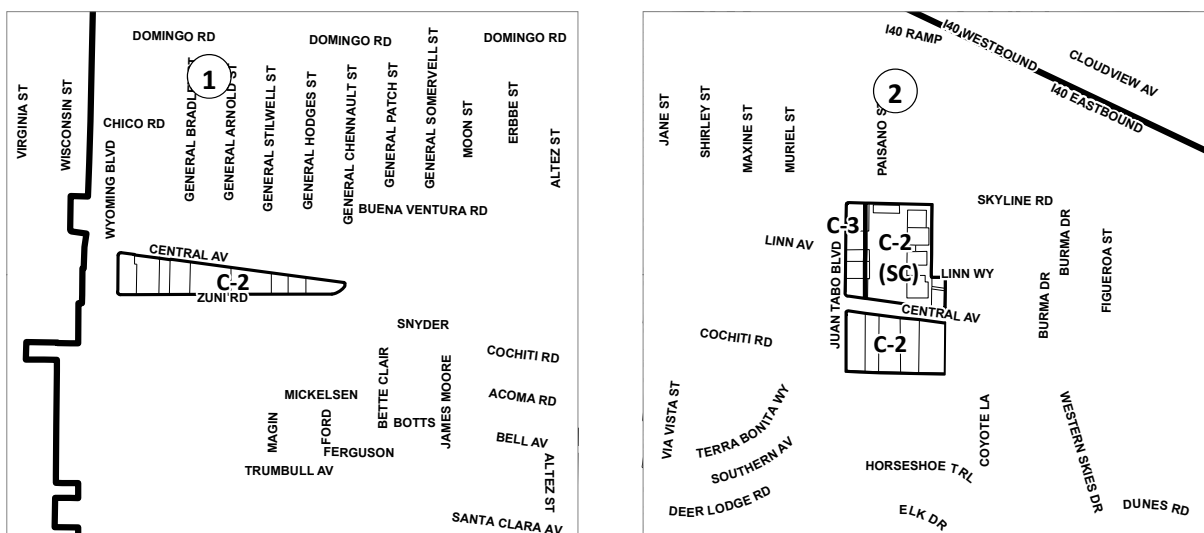


Figure 5.i: Zoning Predating the East Gateway Sector Development Plan

5.4.3 SU-2 East Gateway Corridor Zone (SU-2/EG-C)

ZONE INTENT

The SU-2/EG-C (Corridor Zone) enables the future development of a mixture of non-residential and residential uses that should support a multi-modal environment and encourage legitimate activity along the street between community and neighborhood activity centers. This zoning district is applied to most Plan area properties adjacent to Central Avenue or abutting the east side of Eubank Boulevard from Central Avenue through Southern Avenue. (See maps of these properties following permissive uses of this zone.)

The Corridor Zone allows existing businesses to continue. In addition to a wide variety of commercial uses, it allows residential and manufacturing uses not allowed in these locations before the adoption of this Plan.

PERMISSIVE USES

1. Uses listed as permissive in 14-16-2-17 C -2 Community Commercial Zone, with the following exceptions:
 - a. Antennas are not allowed.
 - b. Parking lots (parking as an individual use rather than required on-site parking) are not allowed.
 - c. Signs: On-premise signs are permitted as regulated in the O-1 Zone 14-16-2-15 and General Sign Regulations 14-16-3-5 of the City Zoning Code except that neon signs are allowed. Off-premise signs are not allowed. Live/work signs may be no more than 8 square feet in area and shall be located on the building wall no higher than the first floor.
 - d. Wireless Telecommunications Facility is allowed only if it is attached to a building, does not exceed building height and is concealed.
2. Townhouses and Apartments as defined in the City of Albuquerque Comprehensive City Zoning Code and controlled in the East Gateway Building Types (Section 5.5 of this Plan) and General Design Regulations (Section 5.6 of this Plan).
3. Live/Work Space in the form of an individual unit, building, or multiple buildings on one premise that are used for both residence and business with residents responsible for the business activity conducted on location. No separation between residential and business activities is required. Live/Work Spaces shall include residences and may include one or more of the following uses:
 - a. Office
 - b. Retail sales of the following goods, plus incidental service and repair and incidental retailing of related goods:
 - Arts and crafts objects and supplies
 - Books, magazines, newspapers, stationery, excepting adult bookstore
 - Cosmetics, notions, hobby supplies
 - Flowers and plants
 - Jewelry
 - Clothing
 - c. Services:
 - Barber, beauty
 - Day care center

- Instruction in music, dance, fine arts or crafts
 - Photography, except adult photo studio
 - Tailoring, dressmaking
- d. Arts and crafts production and manufacturing businesses that comply with federal, state, and local environmental regulations and use 2,000 square feet or less space.
 - e. Residential floor space that is a minimum of 300 square feet per unit for the first residential occupant and 150 square feet per unit for each additional residential occupant.
 - f. Units and buildings shall comply with 14-2-1 Fire Code and currently adopted Building Code for using kilns, centrifugal casting, spray-painting booths, and other art manufacturing processes.
 - g. Odorous, noisy and dangerous processes that are incompatible with living on site are not allowed.
4. Retail business in which products are manufactured, compounded, processed, assembled or treated, as an accessory use, including carpentry, plumbing, sheet-metal working, upholstering, sign painting, making of metal stamps, catering, baking, confectionery making, or jewelry or curio making, provided:
 - a. All activities are conducted within a completely enclosed building.
 - b. Activities or products do not produce odor, dust, smoke, noise, or vibration outside the building.
 5. Manufacturing, assembling, treating, repairing, or rebuilding articles permissive in the M-1 zone, provided:
 - a. All activities are conducted within a completely enclosed building.
 - b. Activities or products do not produce odor, dust, smoke, noise, or vibration outside the building.
 6. Religious Institution
 7. Transit stops and transit facilities

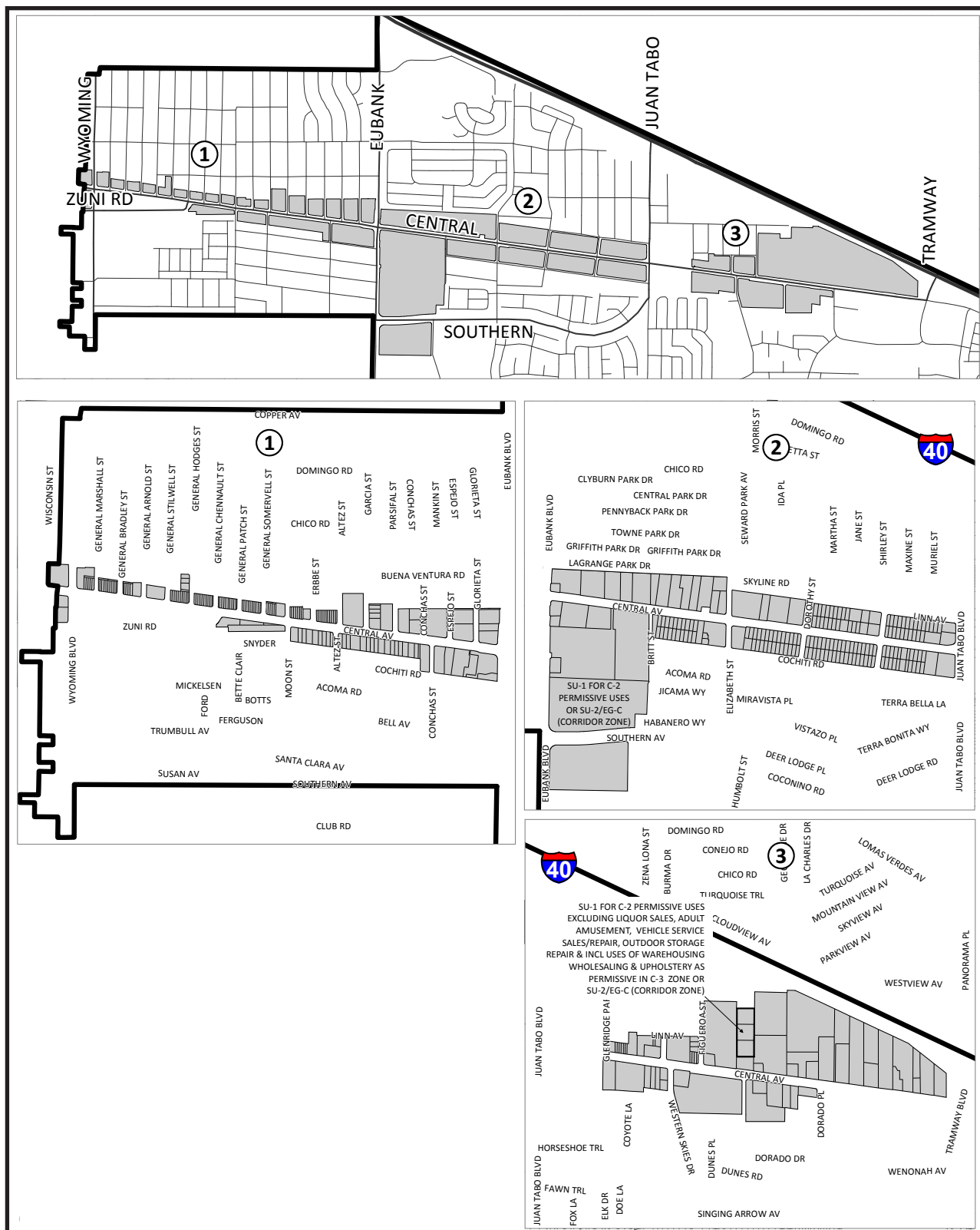


Figure 5.j: East Gateway Sector Development Plan SU-2/EG-C (Corridor) Zone

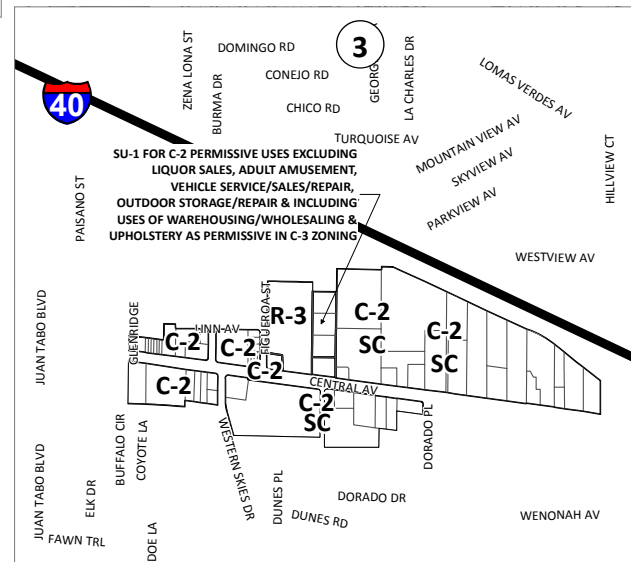
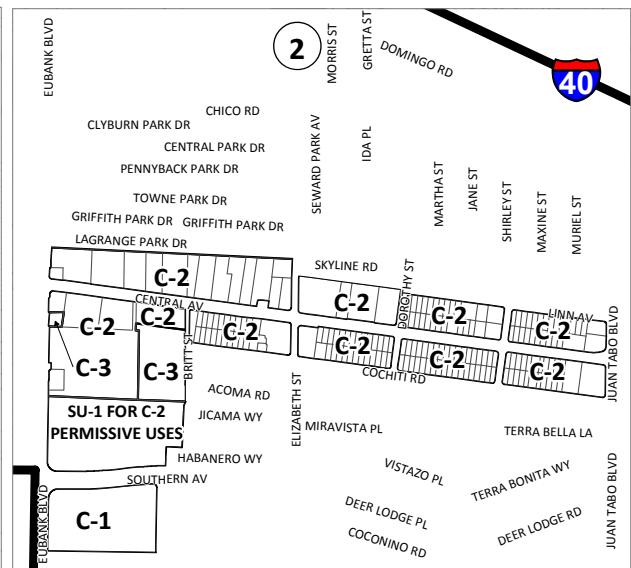
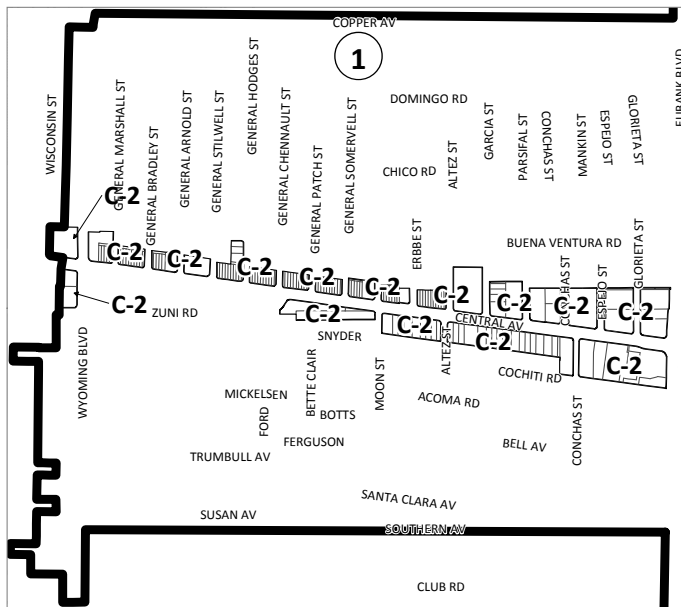


Figure 5.k: Zoning Predating the East Gateway Sector Development Plan

5.4.4 SU-2 East Gateway Community Commercial Zone (SU-2/EG-C-2)

ZONE INTENT

The SU-2/EG-C-2 (Community Commercial Zone) is applied to Plan area properties located away from major arterial streets, but zoned C-2 or SU-1 for community commercial purposes before the Sector Plan was adopted. This zoning district excludes some C-2 uses that are not compatible within residential neighborhoods. It allows existing mobile home parks that were legal nonconforming uses in the C-2 zone to become conditional uses. It also allows additional uses that will provide a better transition between properties lining East Gateway's major streets and residential neighborhoods. (See maps of these properties following conditional uses of this zone.)

PERMISSIVE USES

1. Uses listed as permissive in 14-16-2-17 C-2 Community Commercial Zone, with the following exceptions:
 - a. Alcoholic drink sales are not allowed.
 - b. Antennas are not allowed.
 - c. Circus or Carnival operations are not allowed.
 - d. Parking lots (parking as an individual use rather than required on-site parking) are not allowed.
 - e. Residential uses permissive in the R-3 zone are allowed, but are not required to comply with the C-1 zone minimum acreage requirement.
 - f. Signs: On-premise signs are permitted as regulated in the O-1 Zone 14-16-2-15 and General Sign Regulations 14-16-3-5 of the City Zoning Code. Off-premise signs are not allowed. Live/work signs may be no more than 8 square feet in area and shall be located on the building wall no higher than the first floor.
 - g. Vehicle sales, rental, service, repair, and storage are not allowed.
 - h. Wireless Telecommunications Facility is allowed only if it is attached to a building, does not exceed building height and is concealed.
2. Townhouses and Apartments as defined in the City of Albuquerque Comprehensive City Zoning Code and controlled in the East Gateway Building Types (Section 5.5 of this Plan) and General Design Regulations (Section 5.6 of this Plan).
3. Live/Work Space in the form of an individual unit, building, or multiple buildings on one premise that are used for both residence and business with residents responsible for the business activity conducted on location. No separation between residential and business activities is required. Live/Work Spaces shall include residences and may include one or more of the following uses:
 - a. Office
 - b. Retail sales of the following goods, plus incidental service and repair and incidental retailing of related goods:
 - Arts and crafts objects and supplies
 - Books, magazines, newspapers, stationery, excepting adult bookstore
 - Cosmetics, notions, hobby supplies
 - Flowers and plants
 - Jewelry
 - Clothing

- c. Services:
 - Barber, beauty
 - Day care center
 - Instruction in music, dance, fine arts or crafts
 - Photography, except adult photo studio
 - Tailoring, dressmaking
 - d. Arts and crafts production and manufacturing businesses that comply with federal, state, and local environmental regulations and use 2,000 square feet or less space.
 - e. Residential floor space that is a minimum of 300 square feet per unit for the first residential occupant and 150 square feet per unit for each additional residential occupant.
 - f. Units and buildings shall comply with 14-2-1 Fire Code and currently adopted Building Code for using kilns, centrifugal casting, spray-painting booths, and other art manufacturing processes.
 - g. Odorous, noisy and dangerous processes that are incompatible with living on site are not allowed.
4. Religious Institutions

CONDITIONAL USES

- 1. Mobile Home Parks as regulated in the City Zoning Code in the MH zone. Existing mobile home parks are considered approved conditional uses.



Figure 5.l: East Gateway Sector Development Plan SU-2/EG-C-2 (Community Commercial) Zone

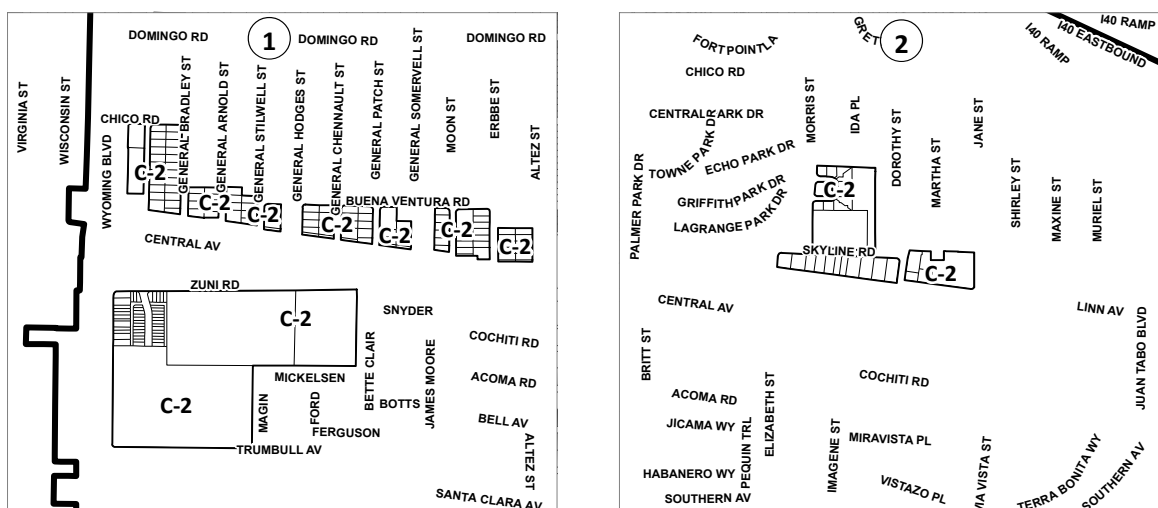


Figure 5.m: Zoning Predating the East Gateway Sector Development Plan

5.5 Building Types

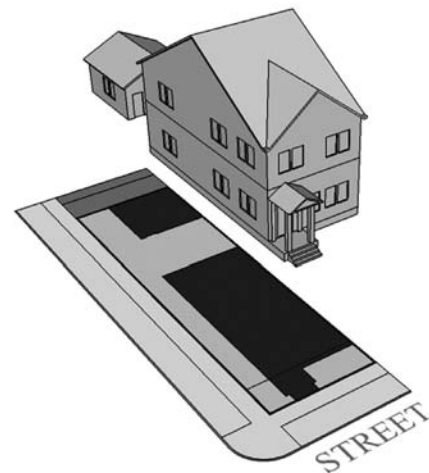
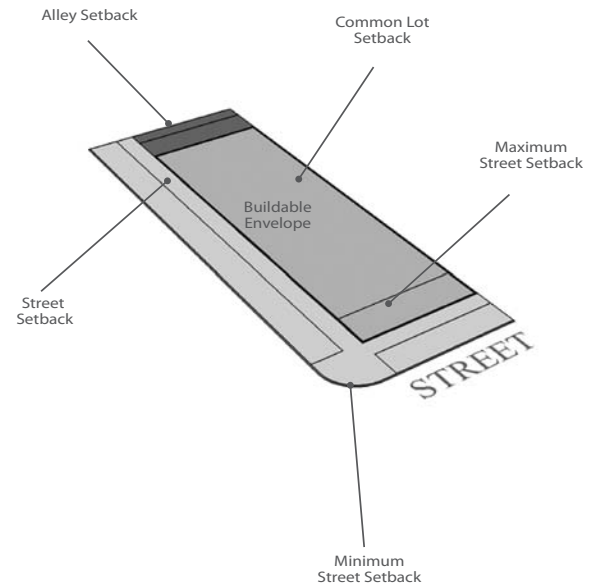
Eleven basic building types are allowed for new development and redevelopment on East Gateway SU-2 zoned properties. The following pages provide details of each building type and the Building Uses, Lot Requirements, Building Lot Coverage, Building Height and Size, On-Site Parking Locations, Building Front Facade Types and Front Yard requirements associated with them.

BUILDING TYPES	COMMUNITY ACTIVITY CENTER ZONE (SU-2/EG-CAC)	NEIGHBORHOOD ACTIVITY CENTER ZONE (SU-2/EG-NAC)	CORRIDOR ZONE (SU-2/EG-C)	COMMUNITY COMMERCIAL ZONE (SU-2/EG-C-2)
Townhouse		x	x	x
Apartment House		x	x	x
Courtyard Apartment Building	x	x	x	x
Apartment Building	x	x	x	x
Apartment Complex			x	x
Live/Work Building	x	x	x	x
Mixed Use Building	x	x	x	x
Commercial Building	x	x	x	x
Liner Building	x	x	x	x
Institutional or Civic Building	x	x	x	x
Industrial Building			x	x

Townhouse Lot

Townhouse lots are located side by side and designed to accommodate a building with more than one dwelling unit. Each dwelling unit is on its own lot and has a private rear yard. Front yards may be shared across lots.

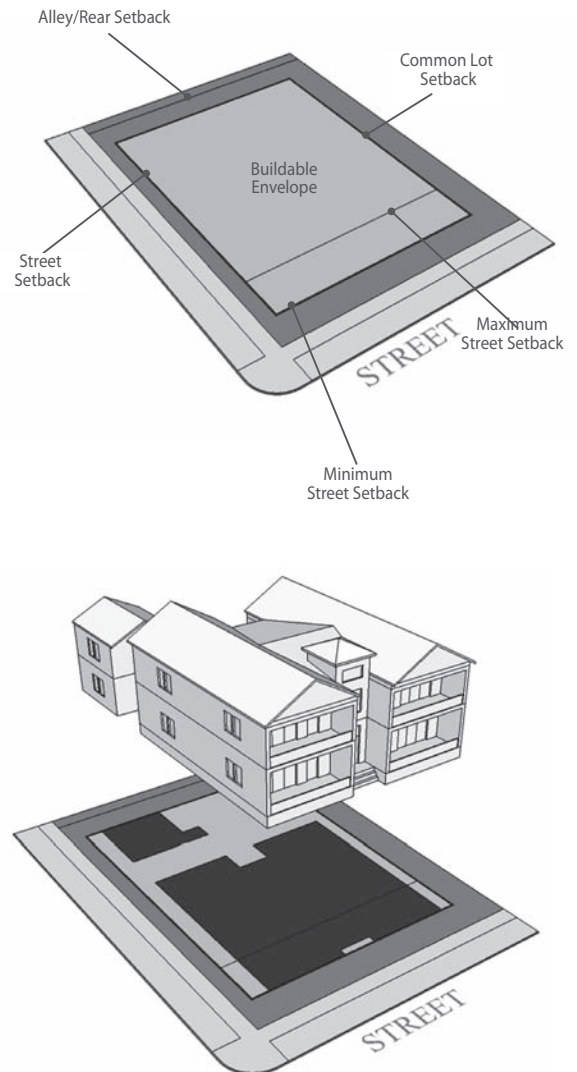
TOWNHOUSE BUILDING AND LOT STANDARDS		
BUILDING USE		
Residential		
LOT REQUIREMENTS	MIN	MAX
Lot Width (linear feet)	18	32
Lot Depth (linear feet)	98	120
Lot Size (square feet)	1,760	3,840
BUILDING LOT COVERAGE	MIN	MAX
Usable Open Space (square feet)	360 per dwelling unit	No Requirement
Lot Coverage (%)	No requirement	80
Building Front Facade Lot Width Coverage (%)	90	100
Front Yard Setback (linear feet)	5	15
Side Yard Setback from property line abutting street right-of-way (linear feet)	10	No requirement
Side Yard Setback from shared lot line (linear feet)	5 from a common property line separating townhouse building and either another townhouse building or another use (See SU-2 General Regulation 5.6.1A.2)	No requirement
Rear Yard Setback (linear feet)	15	No requirement
ACCESSORY BUILDING	MIN	MAX
Setback from Principal Building (linear feet)	10	No requirement
Side and Rear yard setback (See City Zoning Code 14-16-3-3(B)(2) for Accessory Structures)		
Building Footprint (square feet)	No requirement	625
BUILDING HEIGHT linear feet)	MIN	MAX
Principal Building	No requirement	38
Accessory Building(s)	No requirement	26
PARKING LOCATION		
Rear yard for alley access only provided that a minimum 10% of total lot area is maintained as usable open space.		
FRONT BUILDING FACADE AND FRONT YARD TYPE		
Common Yard (no driveways, vehicle storage or movement between front building facade and street)		
Porch and Wall (no driveways, vehicle storage or movement between front building facade and street)		



Apartment House Lot

An Apartment House lot is located and designed to accommodate a building that resembles a large house but contains multiple dwelling units.

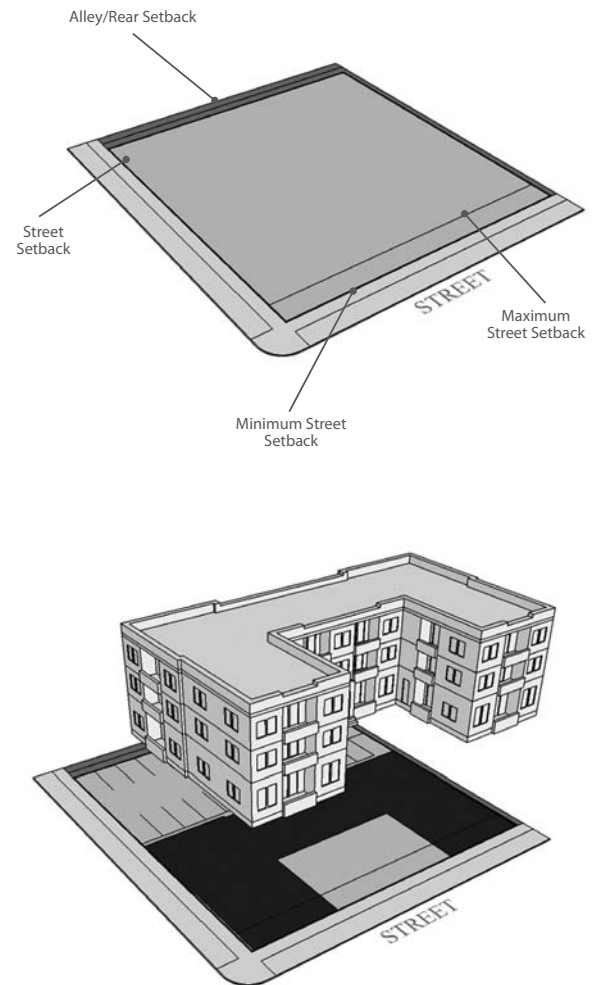
APARTMENT HOUSE BUILDING AND LOT STANDARDS		
BUILDING USE		
Residential		
LOT REQUIREMENTS	MIN	MAX
Lot Width (linear feet)	50	100
Lot Depth (linear feet)	100	150
Lot Size (square feet)	5,000	18,000
BUILDING LOT COVERAGE	MIN	MAX
Usable Open Space (square feet)	400 for each efficiency apt. or one-bedroom apt. 500 for each two-bedroom or larger apt	No requirement
Lot Coverage (%)	No requirement	80
Building Front Facade Lot Width coverage (%)	70	100
Front Yard Setback (linear feet)	5	15
Side Yard Setback from property line abutting street right-of-way (linear feet)	10	
Side Yard Setback from shared lot line (linear feet)	5	
Rear Yard Setback (linear feet)	15	No requirement
ACCESSORY BUILDING	MIN	MAX
Setback from Principal Building (linear feet)	10	No requirement
Side and Rear yard setback (see City Zoning Code 14-16-3-3(B)(2) for Accessory Structures)		
Building Footprint (square feet)	No requirement	625
BUILDING HEIGHT (linear feet)	MIN	MAX
Principal Building	26	38
Accessory Building(s)	No requirement	26
PARKING LOCATION		
In Rear Yard or Side Yard that is not facing a street		
FRONT BUILDING Facade AND FRONT YARD TYPE		
Common Yard (no driveways, vehicle storage or movement between front building facade and street)		
Porch and Wall (no driveways, vehicle storage or movement between front building facade and street)		



Courtyard Apartment Lot

A Courtyard Apartment Building lot is located and designed to accommodate multiple dwellings above or beside each other, facing onto a shared courtyard that is partly or wholly open to the street. Each unit has direct access or shares a common access from a porch, portal or stoop facing the court or the street.

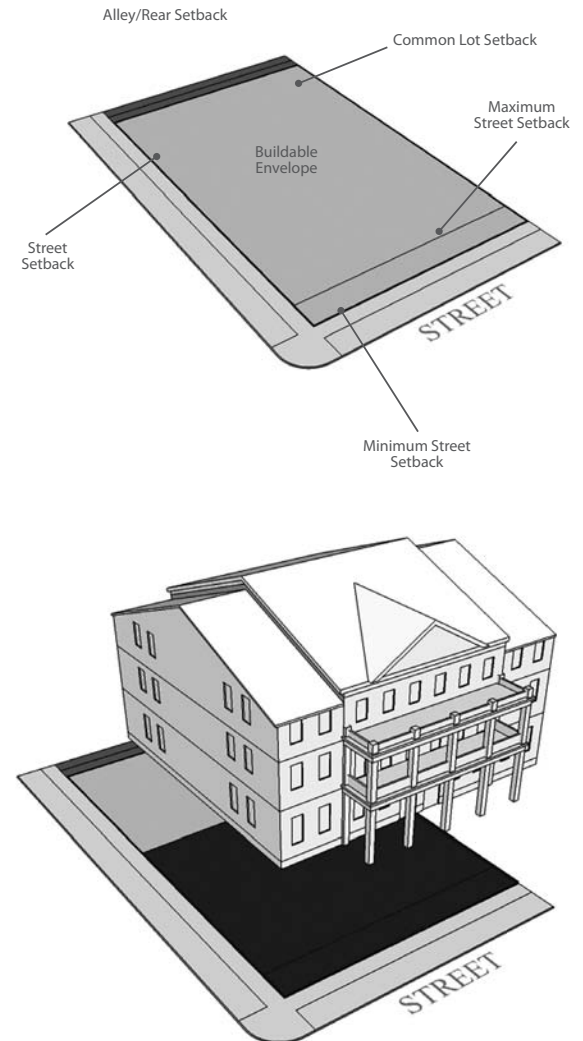
COURTYARD APARTMENT BUILDING AND LOT STANDARDS		
BUILDING USE		
Residential, Hotel or Office		
LOT REQUIREMENTS	MIN	MAX
Lot Width (linear feet)	125	300
Lot Depth (linear feet)	80	300
Lot Size (square feet)	10,000	90,000
BUILDING LOT COVERAGE	MIN	MAX
Usable Open Space	400 for each efficiency apt. or one bedroom apt. 500 for each two-bedroom or larger apt.	No requirement
Lot Coverage (%)	No requirement	80
Building Front Facade Lot Width Coverage (%)	50	100
Front Yard Setback for front facade portions that are not set back inside the courtyard (linear feet)	0	10
Side Yard Setback from property line abutting street right-of-way (linear feet)	0	
Side Yard Setback from shared lot line (linear feet)	5	10
Rear Yard Setback (linear feet)	15	
BUILDING HEIGHT (linear feet)	MIN	MAX
	26	50
PARKING LOCATION		
In Rear Yard or Side Yard that is not facing a street. The courtyard may not be used for parking.		
FRONT BUILDING Facade AND FRONT YARD TYPE		
Forecourt		
Stoop		



Apartment Building Lot

An Apartment Building lot is located and designed to accommodate multiple dwellings above or beside each other in a building that is located close to the front property line and that occupies most of its building lot width.

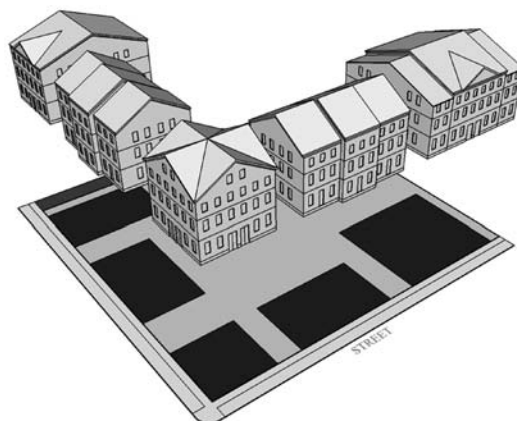
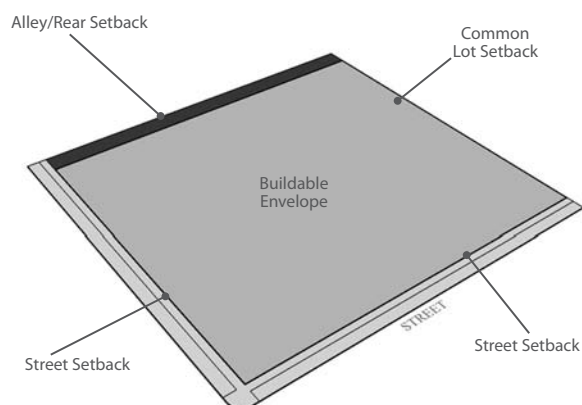
APARTMENT BUILDING AND LOT STANDARDS — not Courtyard		
BUILDING USE		
Residential, Hotel, Office		
LOT REQUIREMENTS	MIN	MAX
Lot Width (linear feet)	40	300
Lot Depth (linear feet)	100	300
Lot Size (square feet)	4,000	90,000
BUILDING LOT COVERAGE	MIN	MAX
Usable Open Space (square feet)	400 for each efficiency apt. or one-bedroom apt. 500 for each two-bedroom or larger apt	No requirement
Lot Coverage (%)		90
Building Front Facade Lot Width Coverage (%)	80	100
Front Yard Setback (linear feet)	0	10
Side Yard Setback from property line abutting street right-of-way (linear feet)	0	
Side Yard Setback from shared lot line (linear Feet)	5	10
Alley/Rear Setback (linear feet)	15	No requirement
BUILDING HEIGHT (linear feet)	MIN	MAX
	26	50
PARKING LOCATION		
In Rear Yard or Side Yard that is not facing a street		
FRONT BUILDING Facade AND FRONT YARD TYPE		
Common Yard		
Porch and Wall		
Forecourt		
Stoop		



Apartment Complex Lot

An Apartment Complex lot is located and designed to accommodate multiple buildings with multiple dwellings above or beside each other.

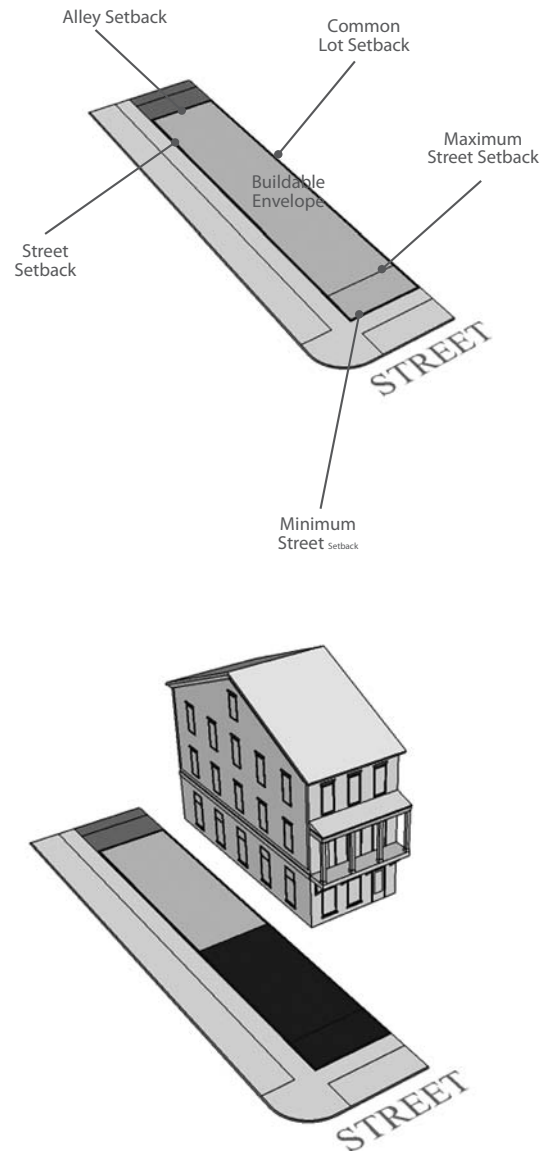
APARTMENT COMPLEX BUILDING AND LOT STANDARDS		
BUILDING USE		
Residential or Hotel		
LOT REQUIREMENTS	MIN	MAX
Lot Width (linear feet)	No Requirement	500
Lot Depth (linear feet)	No Requirement	500
Lot Size (square feet)	No Requirement	250000
BUILDING LOT COVERAGE	MIN	MAX
Usable Open Space (square feet)	400 for each efficiency apt. or one-bedroom apt. 500 for each two-bedroom or larger apt	No requirement
Lot Coverage (%)	No Requirement	75
Building Front Facade Lot Width Coverage (%)	50	100
Front Yard Setback (linear feet)	0	25
Side Yard Setback from property line abutting street right-of-way (linear feet)	0	25
Side Yard Setback from shared lot line (linear feet)	5	No Requirement
Rear Yard Setback (linear feet)	15	No Requirement
BUILDING HEIGHT (linear feet)	MIN	MAX
	No requirement	50
PARKING LOCATION		
In Rear Yard or Side Yard that is not facing a street		
FRONT BUILDING Facade AND FRONT YARD TYPE		
Forecourt		
Stoop		



Live Work Building Lot

A Live/Work Building lot is located and designed to accommodate attached or detached units or buildings with office, retail, arts and crafts production and residential uses.

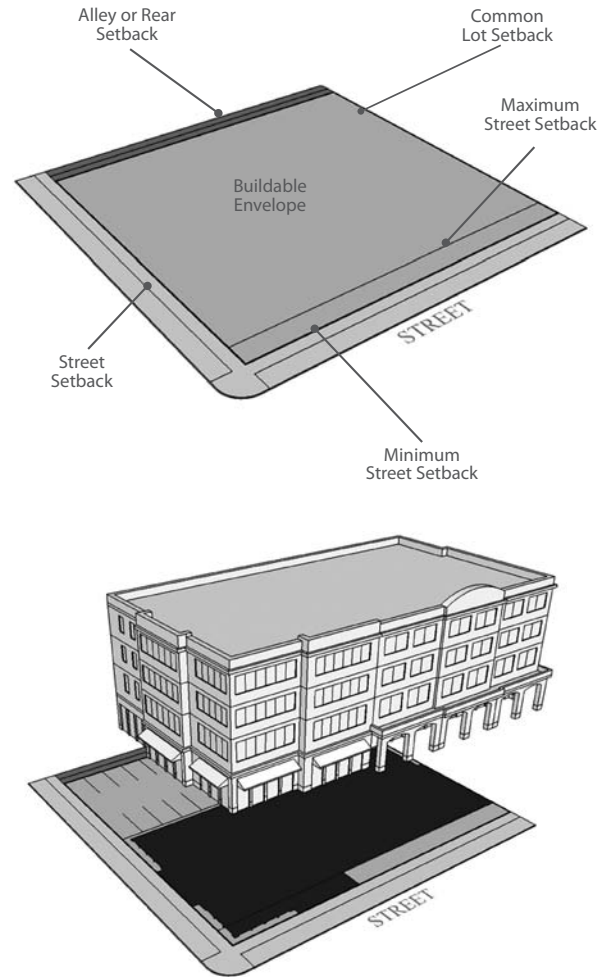
LIVE/WORK BUILDING AND LOT STANDARDS		
BUILDING USE		
Office, Retail, Arts and Crafts Production, Residential		
LOT REQUIREMENTS	MIN	MAX
Lot Width (linear feet)	16	60
Lot Depth (linear feet)	80	120
Lot Size (square feet)	1,280	7,200
BUILDING LOT COVERAGE	MIN	MAX
Usable Open Space (square feet)	80 per unit	No requirement
Lot Coverage (%)	No requirement	80
Building Front Facade Lot Width Coverage (%)	80	100
Front Yard Setback (linear feet)	0	10
Side Yard Setback from property line abutting street right-of-way (linear feet)	0	10
Side Yard Setback from shared lot line (linear feet)	0 from a common property line separating live/work building from a nonresidential use. 5 from a common property line separating live/work building from a residential use or another live/work lot.	
Rear Yard Setback (linear feet)	15	No requirement
ACCESSORY BUILDING	MIN	MAX
Setback from Principal Building	10	No requirement
Side and Rear Yard setback (see City Zoning code 14-16-3-3(B)(2) for Accessory Structures)		
Building Footprint (Square feet)	No requirement	625
BUILDING HEIGHT (linear feet)	MIN	MAX
Principal Building	26	50
Accessory Building(s)	No requirement	26
PARKING LOCATION		
In Rear Yard or Side Yard that is not facing a street		
FRONT BUILDING Facade AND FRONT YARD TYPE		
Store front and Awning		
Arcade		
Gallery		



Mixed-Use Building Lot

A Mixed-Use Building lot is located and designed to accommodate a multi-story building with various commercial uses on the first story and commercial uses and/or dwelling units on upper stories.

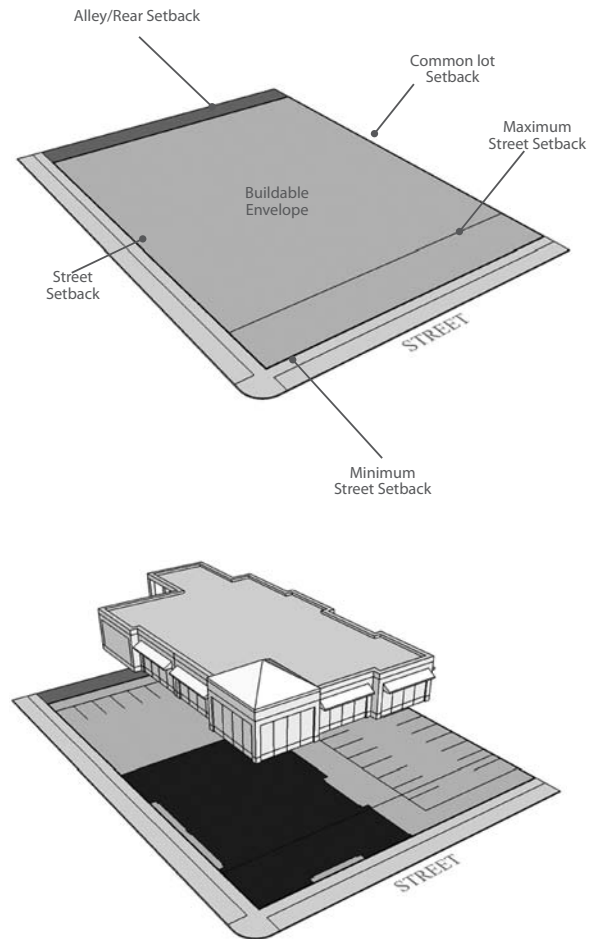
MIXED-USE BUILDING AND LOT STANDARDS		
BUILDING USE		
Hotel, Office, Retail, Residential		
LOT REQUIREMENTS	MIN	MAX
Lot Width (linear feet)	16	300
Lot Depth (linear feet)	No requirement	500
Lot Size (square feet)	No requirement	150,000
BUILDING LOT COVERAGE	MIN	MAX
Lot Coverage (%)	No requirement	90
Building Front Facade Lot Width Coverage (%)	90	100
Front Yard Setback (linear feet)	0	10
Side Yard Setback from property line abutting street right-of-way (linear feet)	0	10
Side Yard Setback from shared lot line (linear feet)	0 if no residential uses. 5 if building contains residential uses or abutting property contains residential uses.	No requirement
Rear Yard Setback (linear feet)	10	No requirement
BUILDING HEIGHT (linear feet)	MIN	MAX
	26	62
PARKING LOCATION		
In Rear Yard or Side Yard that is not facing a street		
FRONT BUILDING Facade AND FRONT YARD TYPE		
Storefront and Awning		
Gallery		
Arcade		



Commercial Building Lot

A Commercial Building lot is located in a retail complex and is designed to accommodate single use office or retail buildings.

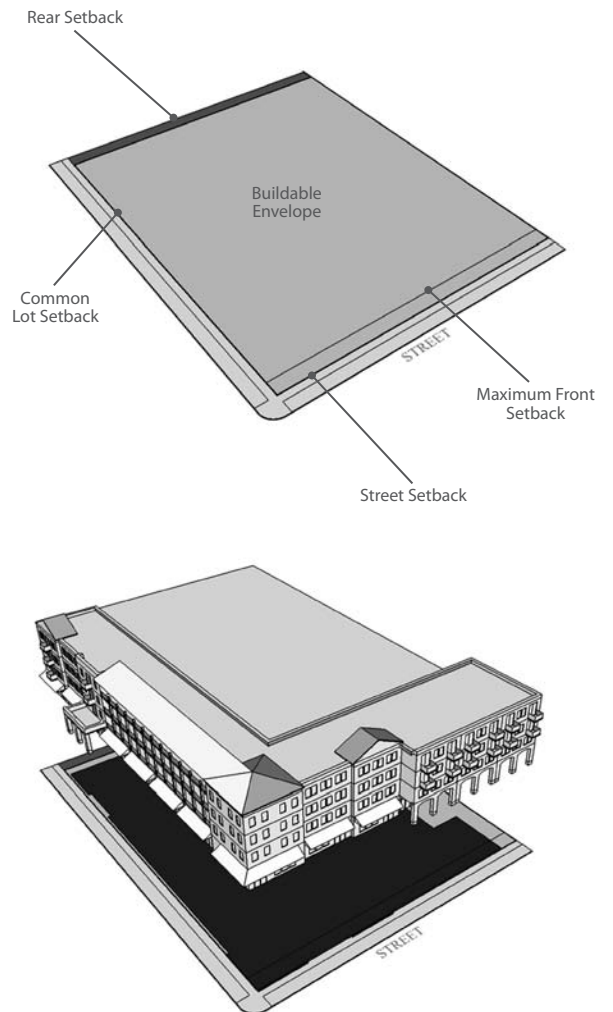
COMMERCIAL BUILDING AND LOT STANDARDS		
BUILDING USE		
Office or Retail		
LOT REQUIREMENTS	MIN	MAX
Lot Width (linear feet)	50	300
Lot Depth (linear feet)	100	300
Lot Size (square feet)	5,000	90,000
BUILDING LOT COVERAGE	MIN	MAX
Lot Coverage (%)	No requirement	60
Building Front Facade Lot Width Coverage (linear feet)	50	100
Front Yard Setback (linear feet)	0	10
Side Yard Setback from property line abutting street right-of-way (linear feet)	0	10
Side Yard Setback from shared property line	0 from a common property line with a nonresidential use. 5 from a common property line with a residential use.	No requirement
Rear Yard Setback (linear feet)	15	No requirement
BUILDING HEIGHT (linear feet)	MIN	MAX
	No requirement	26
PARKING LOCATION		
In Rear Yard or Side Yard that is not facing a street		
FRONT BUILDING Facade AND FRONT YARD TYPE		
Storefront and Awning		
Gallery		
Arcade		



Liner-Building Lot

A Liner-Building lot is located and designed to accommodate a large footprint building such as a parking garage, cinema, supermarket, or other use requiring a large building and a liner building that surrounds the larger building to conceal its blank walls and provide windows and doors that face the street.

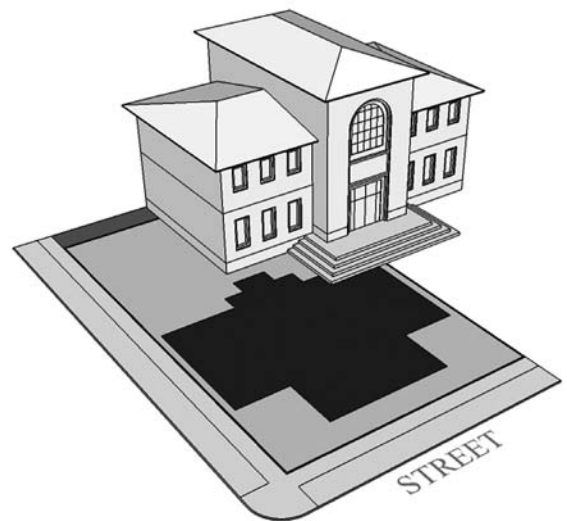
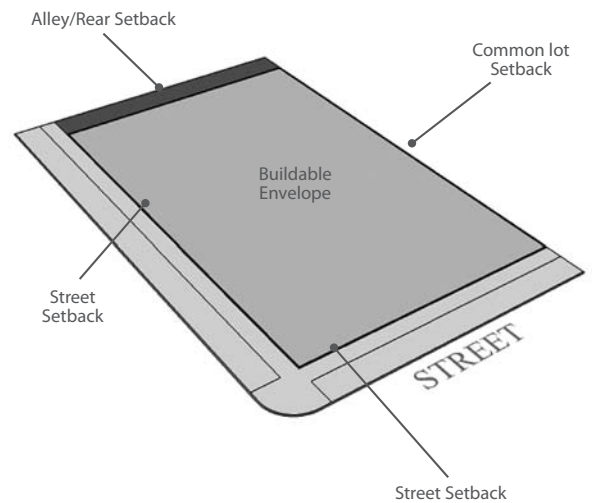
LINER BUILDING AND LOT STANDARDS		
BUILDING USE		
Residential, Office, Retail, Hotel		
LOT REQUIREMENTS	MIN	MAX
Lot Width (linear feet)	No requirement	500
Lot Depth (linear feet)	No requirement	500
Lot Size (square feet)	No requirement	250,000
BUILDING LOT COVERAGE	MIN	MAX
Lot Coverage (%)		90
Building Front Facade Lot Width coverage (%)	90	100
Front Yard Setback (linear feet)	0	5
Side Yard Setback from property line abutting street right-of-way (linear feet)	0	No requirement
Side Yard Setback from shared lot line (linear feet)	0	No requirement
Rear Yard Setback (linear feet)	10	No requirement
LINER BUILDING HEIGHT (linear feet)	MIN	MAX
	No requirement	62
PARKING LOCATION		
In Rear Yard, Side Yard or in attached parking structure		
FRONT BUILDING Facade AND FRONT YARD TYPE		
Storefront and Awning		
Gallery		
Arcade		



Institutional or Civic Building Lot

An Institutional or Civic Building lot is located and designed to accommodate a building containing public uses or quasi-public uses such as day care, education, government services, community services, social services or a religious institution.

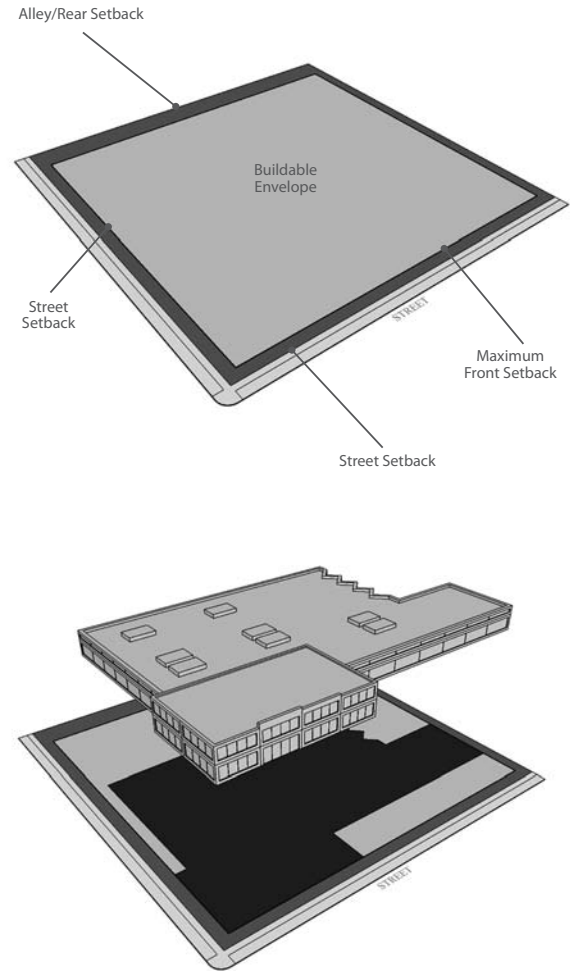
INSTITUTIONAL or CIVIC BUILDING AND LOT STANDARDS		
BUILDING USE		
Institutional and Civic Uses – Fire, Police, Library, Museum, other Public Buildings, and Religious Institutions		
LOT REQUIREMENTS	MIN	MAX
Lot Width (linear feet)		No requirements
Lot Depth (linear feet)		
Lot Size (square feet)		
BUILDING LOT COVERAGE	MIN	MAX
Lot Coverage (%)	No requirement	90
Building Front Facade Lot Width Coverage		No requirements
Front Yard Setback (linear feet)		
Side Yard Setback (Street and shared lot line) (linear feet)		
Rear Yard Setback (linear feet)	10	No requirement
BUILDING HEIGHT (linear feet)	MIN	MAX
	No requirement	50
PARKING LOCATION		
Side Yard or Rear Yard when possible, but not required		
FRONT BUILDING Facade AND FRONT YARD TYPE		
No requirement, however front entrance and windows shall be located on building facade facing the front property line		



Industrial Building Lot

An Industrial Building lot is located and designed to accommodate a building intended for storage or manufacturing.

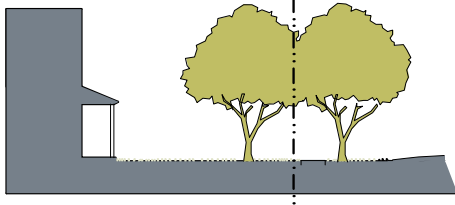
INDUSTRIAL BUILDING AND LOT STANDARDS		
BUILDING USE		
Storage, Manufacturing		
LOT REQUIREMENTS	MIN	MAX
Lot Width (linear feet)	No requirement	500
Lot Depth (linear feet)	No requirement	500
Lot Size (square feet)	No requirement	250,000
BUILDING LOT COVERAGE	MIN	MAX
Lot Coverage (%)	No requirement	75
Building Front Facade Lot Width Coverage (%)	No requirement	No requirement
Front Yard Setback (linear feet)	10	No requirement
Side Yard Setback from property line abutting street right-of-way (linear feet)	10	No requirement
Side Yard Setback from shared lot line (linear feet)	10	No requirement
Rear Yard Setback (linear feet)	25	No requirement
BUILDING HEIGHT (linear feet)	MIN	MAX
	No requirement	38
PARKING LOCATION		
In Rear Yard or Side Yard that is not facing a street		
FRONT BUILDING Facade AND FRONT YARD TYPE		
Forecourt, Storefront and Awning, Gallery or Arcade		



BUILDING FRONT FACADES SHALL FACE THE PUBLIC RIGHT-OF-WAY

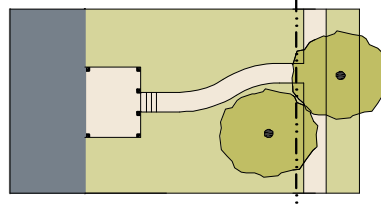
PRIVATE LOT FRONTAGE

PUBLIC R.O.W.

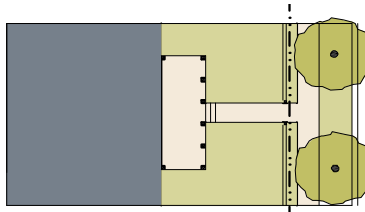
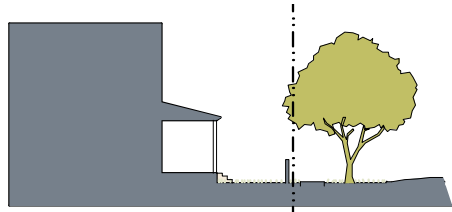


PRIVATE LOT FRONTAGE

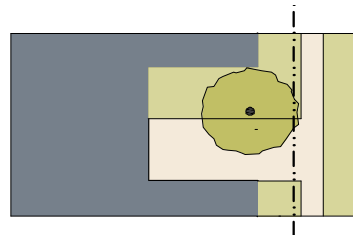
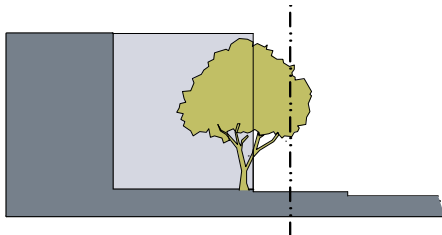
PUBLIC R.O.W.



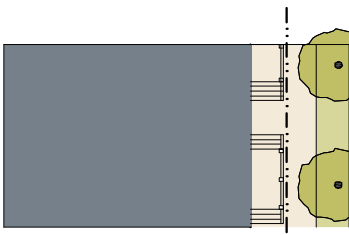
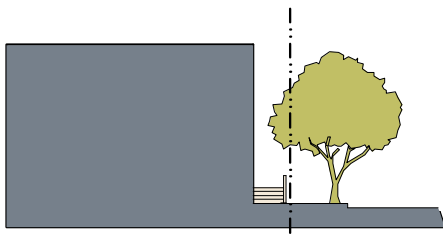
1. **COMMON YARD:** RECOMMENDED FOR TOWNHOUSE, APARTMENT HOUSE, APARTMENT BUILDING AND APARTMENT COMPLEX – Consists of 1) An unfenced landscaped front yard shared by more than one dwelling unit and visually continuous from unit to unit, 2) Deeper building setback distance to provide a buffer from traffic on arterial or collector streets.



2. **PORCH & SHORT WALL OR FENCE:** RECOMMENDED FOR TOWNHOUSE, APARTMENT HOUSE, AND APARTMENT BUILDING – Consists of 1) A short wall or fence at the property line abutting the public right-of-way, 2) Landscaped front yard, 3) Building facade with an attached front porch no less than 8 feet deep.



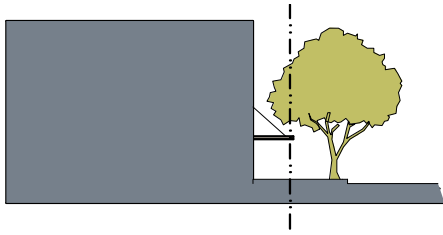
3. **FORECOURT:** RECOMMENDED FOR COURTYARD APARTMENT BUILDING, APARTMENT BUILDING, AND APARTMENT COMPLEX – Consists of 1) A portion of the building facade close to the property line abutting the public right-of-way, 2) Remainder of front building facade set back resulting in a forecourt suitable for vehicular drop-offs or a landscaped courtyard, 3) Large trees within the forecourt that may overhang the public sidewalk



4. **STOOP:** RECOMMENDED FOR COURTYARD APARTMENT BUILDING, APARTMENT BUILDING, AND APARTMENT COMPLEX – Consists of 1) building facade close to the property line abutting the public right-of-way, 2) Building first story elevated above the sidewalk to ensure window privacy, 3) Building entrance accessed by exterior stair and/or ramp and landing.

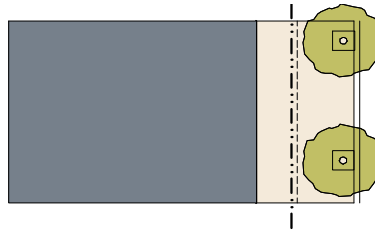
PRIVATE LOT FRONTAGE

PUBLIC R.O.W.

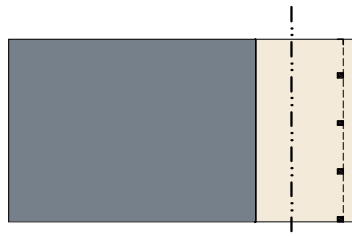
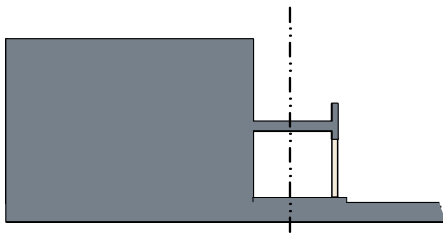


PRIVATE LOT FRONTAGE

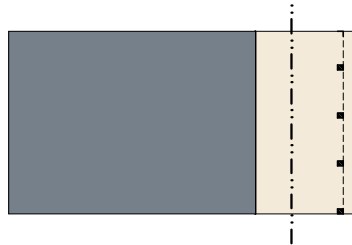
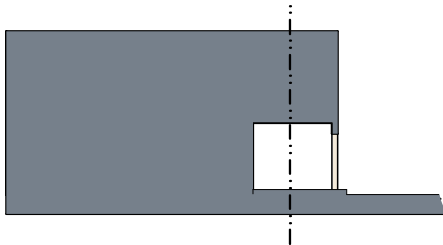
PUBLIC R.O.W.



5. **STOREFRONT WITH AWNING:** RECOMMENDED FOR LIVE/WORK, MIXED-USE, COMMERCIAL, AND LINER BUILDINGS – Consists of 1) Building facade close to the property line abutting the public right-of-way, 2) Building entrance adjacent to public sidewalk, 3) Substantial glazing on first story adjacent to sidewalk, 4) Awnings overhanging the sidewalk by at least 5 feet



6. **GALLERY:** RECOMMENDED FOR LIVE/WORK, MIXED-USE, COMMERCIAL, AND LINER BUILDINGS – Consists of a building facade with an attached minimum 10-foot deep cantilevered shed or light weight colonnade. This portion of the building may or may not extend into the public right-of-way to cover a public sidewalk, but may not extend into the required landscaped sidewalk setback area in the public right-of-way. First story contains many windows.



7. **ARCADE:** RECOMMENDED FOR LIVE/WORK AND MIXED USE BUILDINGS WITH COMMERCIAL USES ON THE FIRST FLOOR – Consists of a building facade with an attached minimum 12-foot deep colonnade supporting habitable space. This portion of the building may or may not extend into the public right-of-way to cover a public sidewalk, but may not extend into the required landscaped sidewalk setback area in the public right-of-way. First story contains many windows.

5.6 General Design Regulations

General Design Regulations for site design, building design, and public right-of-way design are applicable to properties within the General Design Regulations Boundary established on East Gateway Zoning Maps on pages 5-2. Some regulations apply only to properties abutting Central Avenue, some apply only to properties designated as activity centers or abutting major streets, and the rest apply to all properties within the East Gateway General Design Regulations boundary. East Gateway properties outside the General Design Regulations boundary are required to follow regulations in the City of Albuquerque Comprehensive City Zoning Code.

Site Design

5.6.1 Site Layout (Streets, Blocks, and Lots)

The following requirements are intended to ensure a safe, efficient and connected community.

For all East Gateway Properties abutting Central Avenue:

- A. New Central Avenue curb cuts shall be approved only for new intersecting streets necessary to create shorter blocks. New curb cuts for driveways are prohibited on Central Avenue.

For all East Gateway Properties abutting Arterial Streets or within the SU-2/EG CAC, NAC, or C Zones:

- B. Building facades shall occupy at least 50% of the street frontage. If a greater percentage of building facade coverage is required by building type, that requirement prevails.
- C. For blocks longer than 500 feet, intersecting Pedestrian Access Routes (as described in the *Development Process Manual*) shall be built to allow pedestrian and bicycle access a maximum linear distance of every 500 feet. Alleys and Pedestrian Access Routes are not considered block termini.

For all Properties within the East Gateway General Design Regulations boundary:

- D. Rear yards shall not face a public or private street, trail, park, green or square.
- E. Lots abutting or across a public or private right-of-way or alley from lots zoned for or developed with single-family detached housing shall be required to meet the following provisions:
 - 1. New buildings shall be no wider than twice the average single family building width.
 - 2. The space separating each new building shall be a minimum of at least one-half the average width of nearby single-family residential lots.
 - 3. Only the same or similar building types shall be located across public or private rights-of-way from each other. Building types dissimilar from single-family detached houses shall be permitted only when their lots abut a shared

rear lot line or are separated by an alley or service drive. All building types except townhouses and duplexes are considered dissimilar from single-family detached houses.

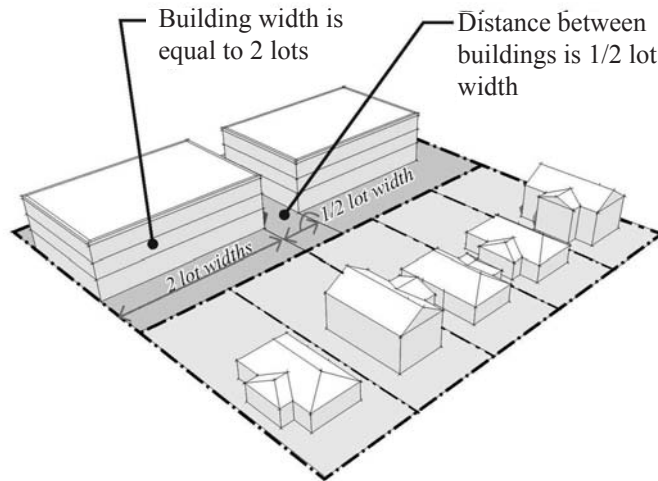


Figure 5.n: Site Layout

- F. Parcels 5 acres and larger shall be designed to provide a block structure that enables efficient pedestrian travel and is defined by an internal network of streets that connect to the existing street network:
1. The entire site shall be planned and platted into maximum 125,000 square foot blocks. Maximum block length is 500 feet.
 2. Primary and secondary driveways (as defined in the City Zoning Code) or platted streets that separate the blocks shall be between 60 feet and 85 feet wide and shall include the following:
 - a. Two 10-foot wide travel lanes;
 - b. Two 5-foot wide landscaped buffers with shade trees spaced approximately 30 feet on center;
 - c. Two 8-foot wide pedestrian walkways constructed of material other than asphalt that meets ADA accessibility guidelines;
 - d. Pedestrian scale lighting that provides at least an illumination of 1.2 to 2.5 foot candles or the equivalent foot lamberts; and
 - e. Standup curb.
- G. Pedestrian Site Circulation and Design
1. All sites shall be designed with well-lit pedestrian pathways physically separated from driveways and parking spaces by landscaping, berms, barriers, grade separations or other means to protect pedestrians from vehicular traffic.
 2. Pedestrian Access Routes (direct pedestrian and bicycle pathways) shall be designed as required in the *Development Process Manual* and provided between buildings and adjacent parcels, and to existing public bicycle trails, lanes, and routes and pedestrian trails and sidewalks. Pedestrian connections shall occur at a minimum distance of 500 feet.

3. Flat, ADA-accessible crosswalks shall be required across all driveways, drive aisles or other vehicular areas.

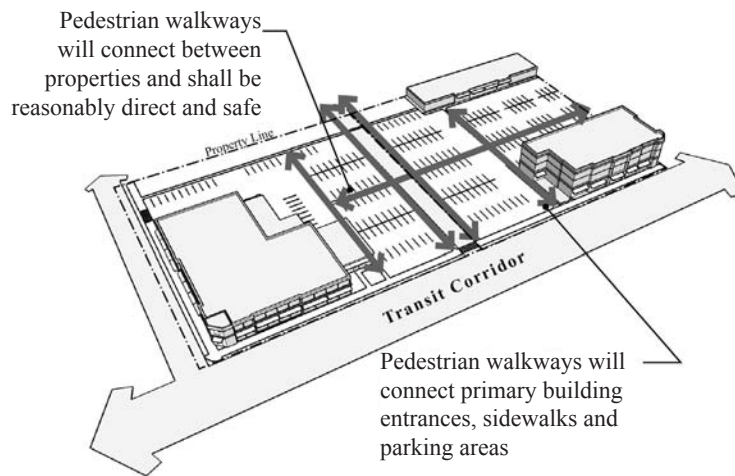


Figure 5.o: Site Layout, Pedestrian Circulation

5.6.2 Parking

For all East Gateway Properties abutting Arterial Streets or within the SU-2/EG CAC, NAC, C and C-2 Zones:

The General Parking Regulations of the City's Zoning Code, Section 14-16-3-1 shall apply, with the following exceptions:

- A. Parking shall be distributed on the site to minimize visual impact from the adjoining street.
- B. The minimum parking requirement shall be the maximum parking allowed, plus 10%.
- C. Shared parking shall be strongly encouraged.
- D. Parking shall be screened from the street by buildings or a combination of 3-foot high wall and 4-foot wide landscape strip.
- E. A 5-foot wide, clear paved path shall be maintained between building and parking areas for pedestrian access.

5.6.3 Parking Structure

For all East Gateway Properties abutting Arterial Streets or within the SU-2/EG CAC, NAC, C and C-2 Zones:

- A. Direct pedestrian access shall be provided from parking structures to each adjacent street.

- B. Liner buildings shall surround the street sides of parking structures unless the structure is separated from the street by another building, or a minimum 30-foot deep landscaped area.
- C. Parking structures shall comply with all building design standards for building facade, mass, scale, building features and lighting as described in the Building Design Section of these SU-2 General Regulations.

5.6.4 Landscaping

For all Properties within the East Gateway General Design Regulations boundary:

The General Landscaping Regulations of the City's Zoning Code, Section 14-16-3-10 shall apply with the following exception:

- A. Water Harvesting Areas: Surface runoff shall be directed into depressed, water collection areas located in landscape areas. The development applicant is required to demonstrate why exceptions may be necessary.

5.6.5 Public Outdoor Space

For East Gateway Properties abutting Central Avenue:

- A. The following general requirements shall apply to all sites abutting Central Avenue:
 - 1. All sites abutting Central Avenue shall provide a minimum 5% of the site as outdoor public space.
 - 2. The Landscaping Zone and the Walking Zone in the Pedestrian Realm, as described in the Street Design Section of these SU-2 General Regulations may be considered for meeting the outdoor public space requirement.
 - 3. All outdoor public space shall connect to pedestrian walkways/sidewalks.
 - 4. A minimum of 15% of the outdoor public space shall be shaded from the summer sun with trees and/or permanent or temporary shade structures.
 - 5. Outdoor public spaces greater than 1,000 square feet shall provide seating.
 - 6. The use of gravel or crusher fines as ground cover is limited to a maximum of 5% of any public outdoor space.
 - 7. If a site contains multiple buildings and/or multiple lots, the required outdoor space may be combined into larger usable outdoor space.
 - 8. The property owner shall maintain outdoor public space.

- B. For sites 5 acres and larger, any of the following shall be considered public outdoor space:
1. A Green is a public space consisting of pathways and landscaping bounded on at least two sides by streets. The space is available for unstructured recreation. Building fronts face all sides of a green.
 2. A Square is a public space consisting of paved walkways, vegetation, trees and ornamental structures such as fountains or gazebos. A square is generally located at the intersection of streets, is bounded on at least two sides by streets and has building fronts facing all of its sides.
 3. Courtyards have at least two of the following elements: landscape, hardscape, water feature, and/or seating. Outdoor seating consists of chairs and benches and may include tables and covered umbrella seating.
 4. A Community garden consists of garden plots available to residents for small-scale cultivation.

5.6.6 Fences and Walls

Walls clearly visible from the public right-of-way that define a site perimeter or that define outdoor spaces within the site shall meet the requirements in §14-16-3-19 of the City's Zoning Code with the following exceptions:

For all East Gateway Properties abutting Central Avenue:

- A. Knee walls (walls 36 inches high or less) shall be located at the street-facing property line to define the property edge where buildings do not.
1. Knee wall height shall be 18 inches minimum and 36 inches maximum.
 2. Gates for pedestrians and vehicles are permitted to interrupt the wall.

For all East Gateway Properties abutting Arterial Streets or within the SU-2/EG CAC, NAC, C and C-2 Zones:

- A. Walls within 20 feet of the property line adjacent to a public right-of-way, excepting walls required to screen outdoor storage, shall not exceed 3 feet in height above grade.
- B. All walls and fences exceeding 3 feet in height that are located within the setback area abutting a public street shall provide variety and articulation at intervals not exceeding 30 feet through either changes in plane or expression of structure such as posts, columns, or pilasters.
- C. All wall sides facing the public right-of-way shall be finished.
- D. The following finish materials are permitted for site perimeter and space defining walls and fences: Native or regional stone or an equivalent imitation stone, metal

(wrought iron, welded steel or aluminum), brick, stucco, or a combination of any two of the above materials. Chain link fencing is not permitted.

- E. Opaque fences or walls shall be used to screen mechanical equipment, loading areas, service areas and outdoor storage.
- F. Temporary construction fences are permitted.

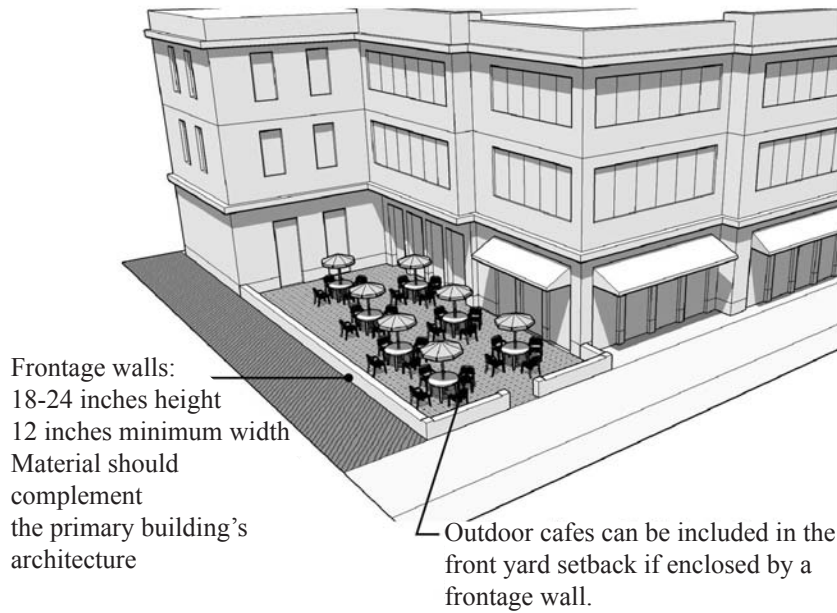


Figure 5.p: Fences and Walls

5.6.7 Service, Loading and Outdoor Storage Screening

For all East Gateway Properties abutting Arterial Streets or within the SU-2/EG CAC, NAC, C and C-2 Zones:

- A. Service, loading and storage areas shall not be visible from any public right-of-way or adjacent residential area.
- B. Service, loading, and outdoor storage areas shall be incorporated into the overall design of the principal building on site using screening walls of compatible material, style, color, texture, pattern, trim and details. A screening wall shall be a minimum of 1 foot higher than the largest object being screened, but not more than 10 feet high on all sides where access is not needed.
- C. Ground-mounted mechanical and electrical equipment shall not be located adjacent to a building's front facade and shall be placed out of view from public or private rights-of-way and residential uses. In addition, the ground-mounted equipment shall be screened from view by fencing, walls, or vegetation in coordination with public and private utility agencies.

- D. Mechanical equipment located on a roof shall be screened from view by parapet walls at least equal to the width and height of the equipment and shall be made of the same materials as the principal building.
- E. Service, loading and emergency service lanes shall be designed as part of site circulation and shall not use dedicated lanes that add impervious surface.
- F. The Recycling Bin Regulations of the City's Zoning Code, Section 14-16-3-15 shall apply.

5.6.8 Lighting

For all Properties within the East Gateway General Design Regulations Boundary:

- A. Lighting shall comply with Section 14-16-3-9 of the City's Zoning Code with the following exceptions:
 - 1. All on-site lighting shall be fully shielded to prevent fugitive light from encroaching onto adjacent properties.
 - 2. Building mounted lighting shall be mounted between 7 feet and 15 feet above grade.
 - 3. All lots abutting alleys shall provide lighting fixtures within 5 feet of the alley right-of-way to illuminate the alley. Maximum fixture height above alley grade shall be 15 feet and shall not cause glare onto adjacent properties. Where possible, lighting shall be attached to a building.
 - 4. Lighting for parking structures shall evenly light the interior space to prevent hiding places.
 - 5. No flashing, traveling, animated or intermittent lighting shall be visible on the exterior of the building.

5.6.9 Signage

For all Properties within the East Gateway General Design Regulations Boundary:

- A. Signage shall comply with Sign Regulations in the SU-2/EG zones of this Plan and Section 14-16-3-5 of the City's Zoning Code with the following exceptions:
 - 1. Off-premise signs are not permitted.
 - 2. Signage may be placed on awnings, canopies and shade structures attached to the building.
 - 3. Signs advertising alcoholic beverages shall be located inside buildings and may not be greater than 4 square feet in area.

4. Signage that abuts or is across the street or alley from property that is zoned for or developed with residential housing shall not be illuminated.

BUILDING DESIGN

5.6.10 Building Materials

For all East Gateway Properties abutting Arterial Streets or within the SU-2/EG CAC, NAC, C and C-2 Zones:

- A. Primary materials for building surfaces shall be stucco, brick masonry, tile masonry, native stone or a synthetic native stone equivalent.
- B. Secondary materials for building surfaces (primarily trim) shall be selected from the following list: tile, pre-cast masonry, gypsum reinforced fiber concrete, metal or wood for beams, lintels and trim, and split-faced block for piers, foundation walls and chimneys.
- C. Exterior material changes shall reflect underlying building construction rather than applied surface decoration that does not relate to the structure.

5.6.11 Building Height

For all Properties within the East Gateway General Design Regulations Boundary:

- A. The Supplementary Height, Area and Use Regulations of the City Zoning Code, Section 14-16-3-3, shall apply.
- B. Structures shall not exceed 26 feet in height within 85 feet of a lot zoned specifically for houses as indicated in City Zoning Code Section 14-16-2-15, the O-1 Zone.

5.6.12 Building Articulation

For all East Gateway Properties abutting Arterial Streets or within the SU-2/EG CAC, NAC, C and C-2 Zones:

- A. A minimum 30-foot wide and 3-foot deep architectural relief shall be required for every 60-foot horizontal length of building facade visible from a street.
- B. A clear visual division shall be provided between the ground level floor and upper floors with a cornice line, awning or other similar feature.
- C. All buildings shall use at least two of the following design features to provide visual relief on building facades:
 1. Divisions or breaks in materials or textures.
 2. Window bays and/or dormers
 3. Multiple entrances with entry treatments distinct from the building facade – recessed or protruding
 4. Variation in roof-lines
 5. Awnings, canopies, overhangs or covered porch entries

- D. Art murals shall not require coordination with the City Arts Program.
- E. Encroachments into the public right-of-way such as awnings, galleries and arcades shall comply with the City's review and approval process for Encroachment Agreements.

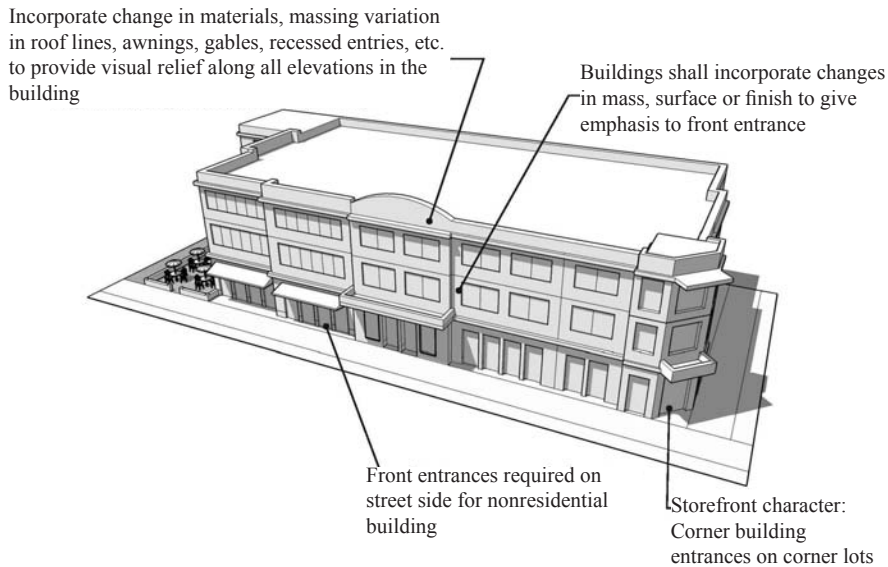


Figure 5.q: Building Articulation

5.6.13 Building Orientation/Entrances

For all East Gateway Properties abutting Arterial Streets or within the SU-2/EG CAC, NAC, C and C-2 Zones:

- A. A primary accessible entrance for every building shall directly face a street, except courtyard buildings where primary entrances may face a central courtyard.
- B. Street-facing entrances shall be clearly defined with facade variations, porticos, roof variations, recesses or projections, or other integral building forms.
- C. Buildings on Corner Lots shall address the street corner in one of the following ways:
 1. Locate the main entrance at the street corner; or
 2. Curve, angle, step back or project the corner of the building at the street corner
- D. Drive-up windows, ordering panels and gas pumps shall be located only at the rear of a building as illustrated below. When rear location is impossible due to site constraints, side location is permissible provided the following requirements are met:
 1. Drive-in facilities (pick up windows, ordering panels, gas pumps) are screened from the public right-of-way and abutting lots by a masonry wall and landscaping. The wall shall be a maximum of 4 feet high on all sides where access is not needed.

2. No portion of queuing or access lanes or driveways shall be located between the building and the street or within 40 feet of the street-facing facades of the building.

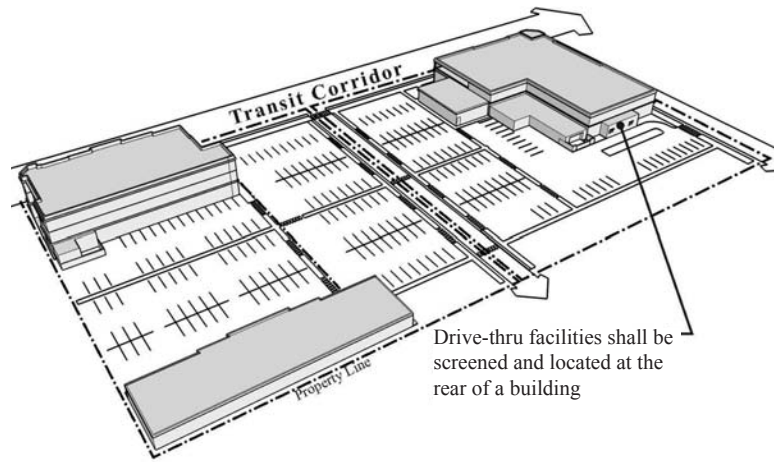


Figure 5.r: Building Orientation/Entrances

3. For corner lots, the drive-in facility shall be located on the side of the building furthest from the corner.
4. Gas station pumps shall be located at the side or rear of the primary building and shall not be visible from the public right-of-way.

5.6.14 Windows and Doors

For all East Gateway Properties abutting Arterial Streets or within the SU-2/EG CAC, NAC, C and C-2 Zones:

- A. Windows and doors shall provide interest on the street and safety for pedestrians using the following standards:
 1. For all building types except residential, industrial, and institutional/civic, a minimum of 50% of the area of the ground story between the sidewalk elevation and 9 feet in height above it shall be windows that are clear and allow light transmission between exterior and interior.
 2. For all building types except industrial and institutional/civic, a minimum of 30% of the area of each floor above the ground story shall be windows.
 3. Horizontal dimensions of windows and doors shall not exceed vertical dimension.
 4. Doors and windows shall correspond to interior space and shall not span across floors.
 5. Maximum windowsill height for non-residential buildings shall be 36 inches above sidewalk elevation.
 6. Windows over 10 square feet shall be divided into multiple panes to enhance the building facade.

7. Industrial buildings shall be located and designed to minimize the impact of windowless walls and service areas or be required to line the street sides of the building with liner buildings or windows.
- B. Commercial, Mixed Use and Liner Buildings shall express a storefront character using the following elements:
1. Regularly spaced and similar-shaped windows with trim on all building stories
 2. Display windows on the ground floor of all building facades facing streets, parks and plazas covering a minimum of 50% and a maximum 80% of the surface area. The bottom edge of display windows on the ground floor facades shall be no more than 36 inches above sidewalk elevation. Mirrored glass, obscured glass and glass block shall not count toward the minimum display window required.

Public Right-of-Way Design

5.6.15 Central Avenue Street Design

- A. New development and redevelopment triggering other East Gateway Sector Development Plan land regulation compliance also trigger compliance with regulations listed for the Pedestrian Realm of Central Avenue. Regulations for the Roadway Realm shall apply to public projects. Central Avenue shall be redesigned as a major transit corridor as indicated in the Great Streets Facility Plan, April 2009 draft.

B. Roadway Realm Design

1. Vehicular Lanes
 - a. A maximum of four through lanes is preferred, but six is allowed.
 - b. The lane adjacent to the median is a maximum of 10 feet wide.
 - c. Left turn lanes are located no more frequently than on alternate blocks and are 10 feet wide.
 - d. The center turn lane may be eventually replaced with medians.

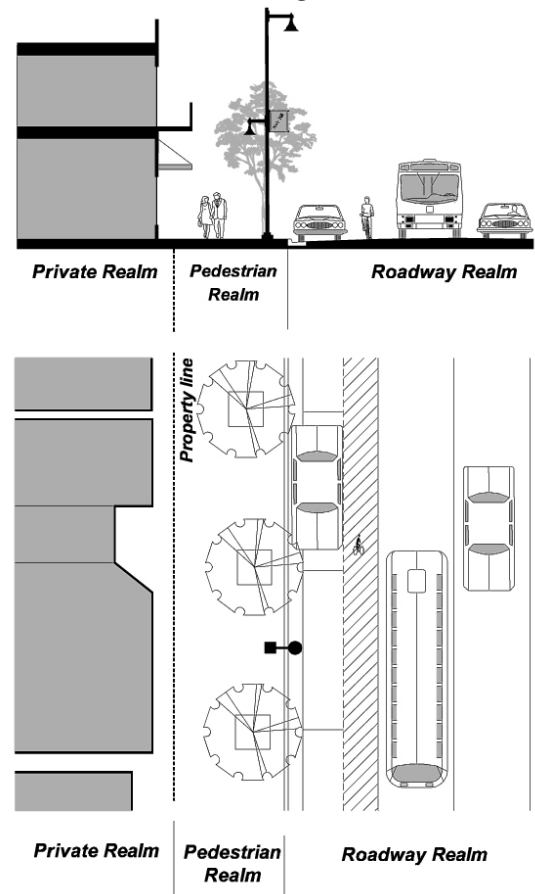


Figure 5.s: Roadway Realm Design

2. Speed Limit shall be a maximum of 35 mph and preferably 30 mph to support pedestrian safety.
3. Parallel parking shall be allowed in select locations.
4. All public utility facilities easements and rights-of-way shall be coordinated with appropriate public and private agencies.
5. Medians
 - a. Median widths shall be a minimum of 6 feet wide and a maximum of 18 feet wide.
 - b. Median landscaping shall include small to medium sized trees to allow panoramic views to the east and west while providing vertical elements to reduce the perceived width of the street.
 - c. Pedestrian refuges shall be provided at all signalized street intersections (6-8 feet wide x the width of the median)
 - d. Pedestrian refuges shall be provided at all unsignalized street intersections (8-10 feet wide x the width of the median).
6. Crosswalk Width
 - a. At all signalized street intersections: 10-12 feet wide, yellow or white stripes in direction of traffic outlined by white lines and white stop bars
 - b. At all non-signalized intersections: Pedestrian caution signs only
7. The pedestrian network shall provide direct pedestrian and bicycle pathways to surrounding buildings, adjacent parcels, all neighborhoods, public and private schools, and parks and recreational spaces greater than or equal to 5 acres in size within a corridor.
8. Street Lights
 - a. The Pole Style and Luminaire Style standard shall be established by the City to be used throughout East Gateway.
 - b. Lights shall be placed 2 feet from back of curb on each side of the street and travel lanes.

C. Pedestrian Realm

1. The public pedestrian realm shall include the public utility easement, edge zone, landscaping zone and walking zone. Where right-of-way is constrained, the edge and landscape zones may be combined provided that a minimum 6-foot-wide clear walking zone sheltered from the street with a landscape zone remains. A 10-foot wide walking zone is preferable.
 - a. Public Utility Easement Width (PNM and others) shall be a minimum of 10 feet.
 - b. Edge Zone shall be a minimum of 2 feet wide to accommodate parking meters, regulatory signs and wayfinding signs.

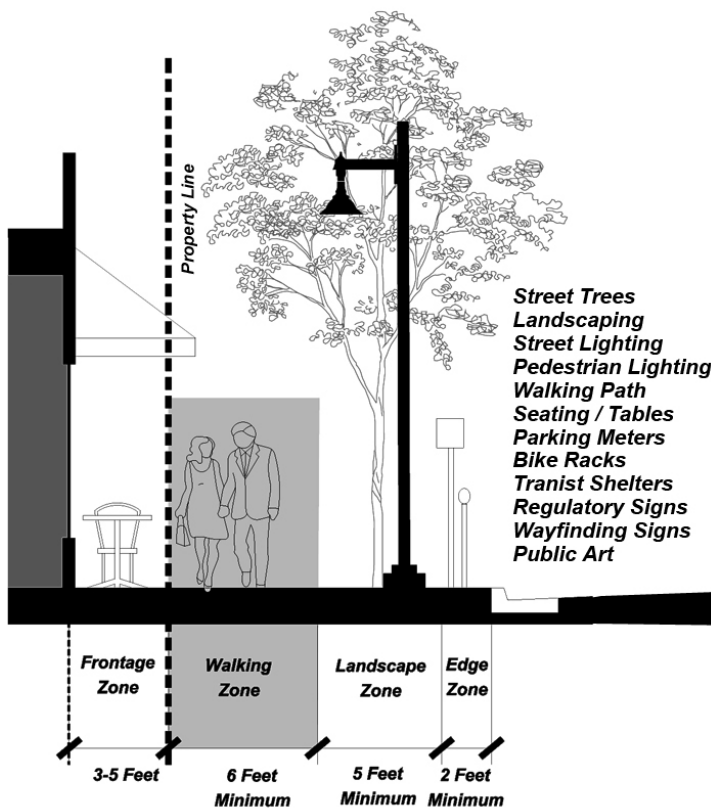


Figure 5.t: Pedestrian Realm Design

c. Landscaping Zone shall be a minimum of 7 feet wide, but may include the 2-foot wide edge zone. The landscaping zone shall be located between the clear walking zone and the curb:

- Street trees shall be a maximum height of 25 feet at maturity if under utility wires or up to 45 feet high in vertically unconstrained areas.
- Transit stops shall be 90 feet long at signalized street intersections and 50 feet long mid-block and at unsignalized street intersections.
- Street Lighting shall be 30 feet high.
- Pedestrian Lighting shall be 12-15 feet high.

d. Walking Zone width shall be a minimum of 6 feet on Central Avenue, however a 10-foot or greater width is preferable:

- Cross slope shall be 1% or less including sections over driveways.
- Running slope maximum shall be 5%.
- ADA ramps shall be directional.

6 The Plan – Transportation

6.1 Introduction

The following recommendations are intended to improve walking, cycling, transit use and driving conditions in East Gateway. Implementing these recommendations and regulations contained in Chapter 5 should enable more East Gateway residents, employees and visitors to be out and about using East Gateway stores, services, and recreation.

This chapter recommends 1) Projects to improve the East Gateway portion of the City bicycle network, 2) Park and Ride/Transit Transfer Center site selection and development, and 3) Street project proposals to improve traffic movement during peak traffic periods, control driver behavior, beautify the area, and enable safe and efficient pedestrian and cyclist travel.

Design regulations for Central Avenue between the curb and building frontages (Pedestrian Realm) will be implemented with development projects abutting Central Avenue right-of-way. Design regulations found in Chapter 5 of this Plan are based on the April 2009 Great Streets Facility Plan draft.

Plan Appendix C includes Central Avenue design proposals that the community may wish to discuss in the future. Due to a lack of consensus concerning the effect of the proposals on vehicular traffic, this Sector Development Plan does not propose major street redesign for Central Avenue.

6.2 Plan Goals Addressed by Recommendations

Goal 3 - Design and build a network of streets and trails that offer multiple efficient, safe transportation choices: driving, cycling, walking, public transit.

Goal 4 - Transform Central Avenue into a vibrant place that functions as a community destination — a seam rather than a barrier.

6.3 Bicycle Network Recommendations

This Plan recommends 1) Bicycle Boulevard development for several streets; 2) Bicycle safety improvements at key bicycle street crossings; 3) A new bicycle lane segment to fill a bicycle network gap; 4) Trail entrance improvements at the Sandia Vista Park entrance to the I-40 Trail, the eastern entrance to Singing Arrow Park, and the Gallant Fox Road/Raton Avenue bicycle entrance; and 5) Consistent I-40 trail maintenance. Although adding bicycle lanes on Central Avenue was discussed, the community did not wholly support them during Sector Plan development. The community may wish to discuss bicycle lanes and Central Avenue design in the future. (See Appendix C.)

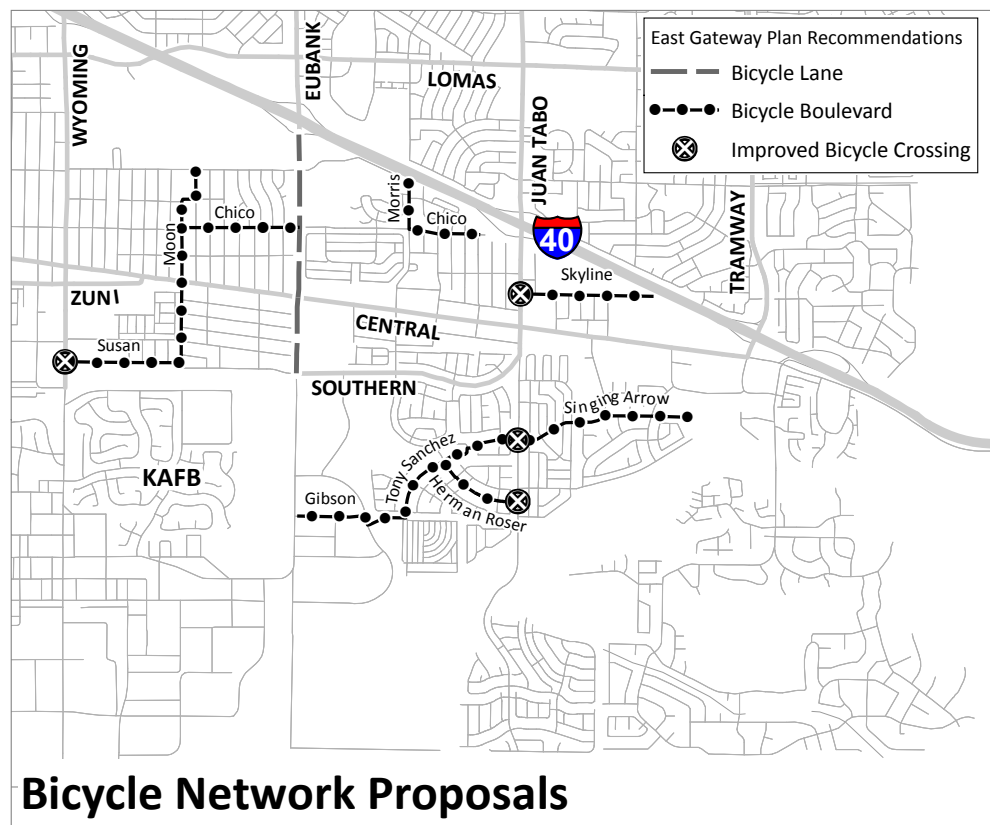


Figure 6.a: Bicycle Network Proposals

6.3.1 Bicycle Boulevards/ Traffic Calming

This Plan recommends Bicycle Boulevard designation and design to improve and complete portions of the bicycle network on Chico and Skyline Roads, Moon Street, Susan Street, Morris Street, Singing Arrow Avenue, Herman Roser Street, Tony Sanchez Street and Gibson Boulevard. Some of these streets are planned or existing bicycle routes. The Bicycle Boulevard designation and design will make them more usable by calming traffic. (See the map above.)

A Bicycle Boulevard is a shared roadway optimized for bicycle traffic. Bicycle Boulevards discourage cut-through motor vehicle traffic, but allow local traffic. They are designed to give priority to cyclists as through traffic. Experienced cyclists enjoy lower traffic volumes without significant increases in trip time and less experienced cyclists use them to gain some experience sharing the roadway with motorized vehicles. Chico and Skyline Roads are wide enough to accommodate striped bicycle lanes.



Bicycle Boulevard traffic-calming elements help achieve a safe environment:

- Low traffic volumes
- Only local vehicular traffic
- Free-flow travel for bicycles
- Safe arterial street crossings
- A distinctive look to encourage cyclist use and alert motorists that the street is a priority route for cyclists

(See Section 6.5.7 of this chapter for information on traffic-calming techniques.)

6.3.2 Arterial Street Crossing Safety Improvements

This Plan recommends traffic signal installation at the Central Avenue intersection with the Elizabeth Street Bicycle Lane. (See figure 6.g.) The traffic signal would serve vehicular traffic, pedestrian traffic and a bicycle lane and route that connect neighborhoods south of Southern Avenue, the Manzano Mesa Multi-Generational Center at Southern Avenue, bus service on Central Avenue, neighborhoods north of Central Avenue, the I-40 Bicycle Trail, an I-40 Bicycle overpass, and Tomasita Elementary School and Kennedy Middle School just north of I-40.

The Plan also recommends that the City evaluate arterial street crossings for each East Gateway bicycle route, boulevard, lane, and trail to determine the appropriate type of safe crossing features needed; then program, fund and install them.

Some safe crossing features to consider are as follows: median refuges, traffic control devices including new or upgraded traffic signals, pavement markings, traffic stripes, in-roadway crossing lights, flashing beacons, bicycle-sensitive signal actuation devices, pedestrian countdown signals, vehicle speed feedback signs, pedestrian-activated signal upgrades, and sight distance improvements.

6.3.3 Other Bicycle Network Links

This Plan recommends completing the bicycle lane on Eubank Boulevard from the I-40 overcrossing to Southern Avenue.

6.3.4 Trail Entrances

To increase personal safety, the Plan proposes that the City evaluate all East Gateway trail entrances for cyclist safety and then plan and implement improvements to make entrances highly visible from adjoining properties or public rights-of-way. This Plan recommends that the City negotiate with the New Mexico Department of Transportation to acquire land between Sandia Vista Park and the I-40 trail to develop as part of the park with better access to the I-40 Bicycle Trail.

6.3.5 Trail Maintenance

The City Parks and Recreation Department has many miles of bicycle trails to maintain. Gravel, broken glass and other dangerous impediments to riding need to be regularly swept. Perhaps volunteers and the City Parks and Recreation Department can work jointly to accomplish regularly scheduled maintenance.

6.4 Transit Recommendations

Albuquerque's new Rapid Ride transit service to East Gateway service began August 2009. It provides 15-minute service at stops located at major Central Avenue intersections (Wyoming, Eubank, Juan Tabo and Tramway Boulevards).

Park and Ride /Transit Transfer Center

The City uses a shopping center overflow parking area located at Wenonah Avenue and Tramway Boulevard just south of Central Avenue as an interim park and ride lot. The City began renting the space during Sector Plan development. Waiting buses use a former turning lane on Tramway Boulevard and a westbound lane on Wenonah Avenue across the street from a residential apartment complex. A bicycle trail entrance to Singing Arrow Park is located at the southern end of the park and ride lot.

This Sector Plan recommends that the City select, acquire and develop a site for an integrated park and ride lot and bus transfer station. The site needs to be visible from the street, safe, and highly accessible for pedestrians and cyclists. Idling buses should be located away from residential buildings. The site needs good vehicular access from Central Avenue and pedestrian proximity and access to East Gateway services and shopping. The City Transit Department indicates that locations as far west as Western Skies Drive would work for transit operations. The Rapid Ride bus route would still terminate at Tramway Boulevard or Four Hills Road.

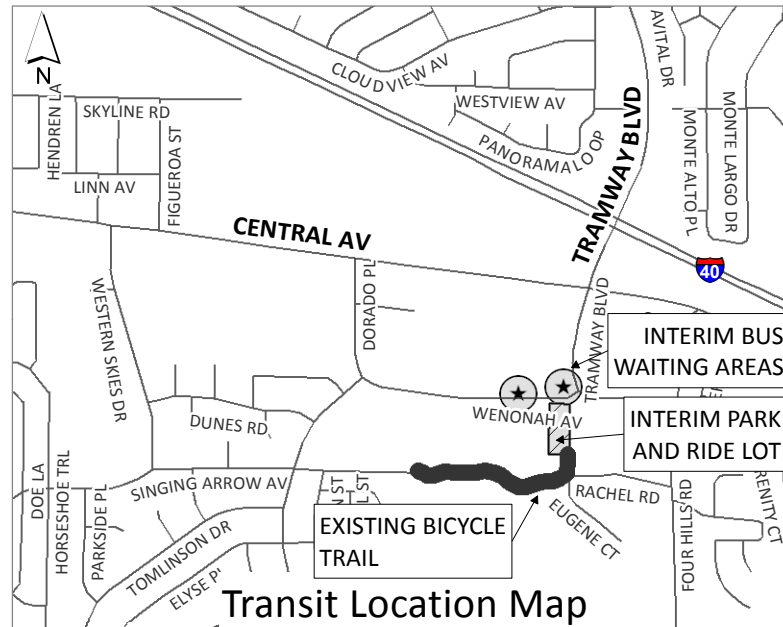


Figure 6.b: Transit Location Map

6.5 Street Project Recommendations

This plan recommends street projects to 1) Improve vehicular traffic movement at key street intersections, 2) Extend a Central Avenue spot median project, 3) Landscape Wyoming Boulevard medians to calm traffic and improve area aesthetics, 4) Redesign Southern Avenue to improve traffic flow and finish the street, 5) Improve traffic movement at the Southern Avenue/Juan Tabo Boulevard intersection, 6) Create safe pedestrian crossings at all signalized street intersections and bicycle street crossings, and 7) Use traffic calming techniques on proposed Bicycle Boulevards.

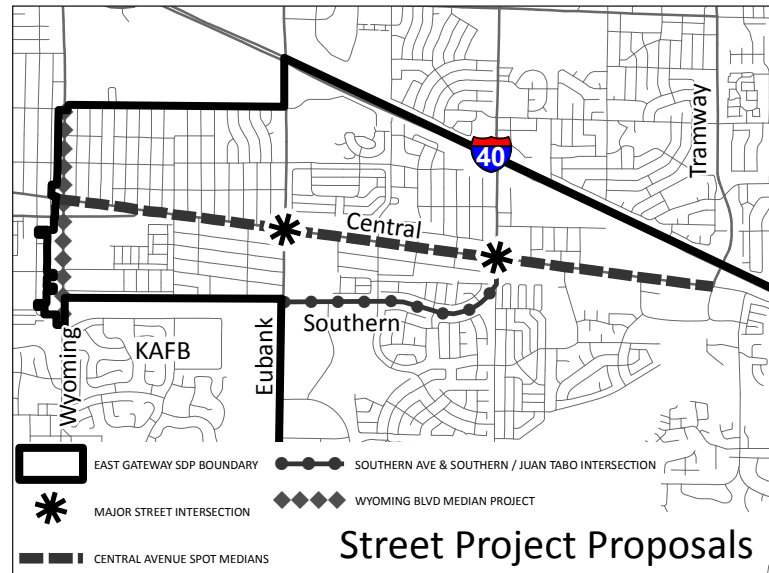


Figure 6.c: Street Project Proposals

6.5.1 Central Avenue Major Street Intersections

This Plan recommends evaluating turning movements and constructing street projects to add turning lanes to alleviate rush hour vehicular congestion and delay at Central Avenue/Eubank Boulevard and Central Avenue/Juan Tabo Boulevard intersections.

- **Central Avenue/Eubank Boulevard Intersection**
The project is in the design phase with construction scheduled for 2012.
- **Central Avenue/Juan Tabo Boulevard Intersection**
This Sector Plan recommends funding and programming a project to study, design and build improvements at this intersection in conjunction with design for Southern Avenue and the Juan Tabo Boulevard/Southern Avenue intersection.

6.5.2 Central Avenue Spot Medians

During the Sector Plan process, a City project was initiated to design, build and landscape spot medians at key locations on Central Avenue. The medians were positioned to create the least disruption to turning movements from Central Avenue to streets and driveways between Elizabeth Street and Tramway Boulevard.

This Sector Plan recommends initiating a second phase of this project on Central Avenue between Eubank and Wyoming Boulevards. This Plan also recommends that the turning lane between medians be colored to differentiate it from through-traffic lanes. These two changes will improve driver behavior and vehicle flow. The medians may also create some pedestrian and bicycle refuges.

If future land development creates a regular pattern of cross streets and blocks, this too will influence driving behavior on Central Avenue. Then the community may determine that further public right-of-way investment would help to add vitality to the street and the community.

6.5.3 Wyoming Median Project

This Plan recommends landscaping the existing Wyoming Boulevard medians between I-40 and Central Avenue to enhance corridor aesthetics and calm traffic speeds. The Plan also recommends that median noses be extended where possible to create pedestrian refuges at intersections with cross streets.

The Plan recommends replacing the continuous left turn lane south of Central Avenue with spot medians and a textured and colored left turn lane. Very few southbound left turns are needed south of Zuni Road.

6.5.4 Southern Avenue Project

The Plan recommends providing four continuous travel lanes on Southern Avenue — two in each direction between Eubank and Juan Tabo Boulevards. Travel demand forecasts for the next twenty years indicate that Southern Avenue will not carry traffic volumes to justify more than four lanes.

The recommendation for Southern Avenue between Elizabeth Street and Juan Tabo Boulevard is to leave the wide median and design the street as a parkway. Between Eubank Boulevard and Elizabeth Street, the recommendation is to narrow the median to capture land for development on the south side of Southern Avenue. Redesign should include median openings at Stephen Moody Boulevard to allow left turns and better neighborhood access to and from shopping at Eubank.

The Plan also recommends evaluating whether Britt Street (just east of Eubank Boulevard) should be widened where it exists between Central Avenue and Acoma Street and extended south to intersect with Southern Avenue to improve transportation route options in this area.

In 2009, the District 9 City Councilor requested that City Council support a request to the Metropolitan Transportation Board to reclassify Southern Avenue as an urban collector street and remove its proposed limited access status. This would improve access to existing and proposed businesses and neighborhoods near the Eubank Boulevard/Southern Avenue intersection. Collector streets are usually narrow. When reclassification is executed, it should include a special exception to accommodate Southern Avenue's existing right-of-way. The Metropolitan Transportation Plan no longer includes Southern Avenue as part of a loop road system requiring limited access.

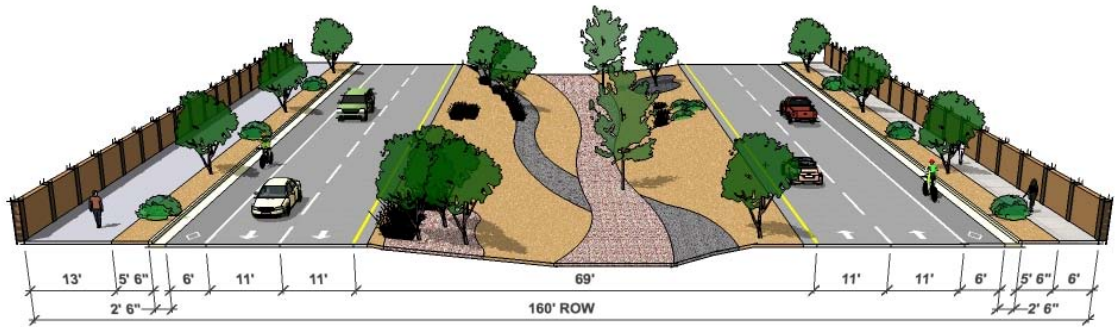


Figure 6.d: Southern Avenue Proposal (Elizabeth Street to Juan Tabo Boulevard)

6.5.5 Southern Avenue/ Juan Tabo Boulevard Intersection

This Plan proposes that a roundabout be evaluated as one possible option to improve traffic operations at the Southern Avenue/Juan Tabo Boulevard intersection. A roundabout would allow room for a landscaped entrance to Juan Tabo Boulevard or a park for the surrounding neighborhoods. The large, triangular right-of-way existing at this intersection in 2009 was intended to accommodate free-flowing vehicular movement from southbound Juan Tabo Boulevard to westbound Southern Avenue. A roundabout would allow greater traffic flow from both streets without the delays that currently occur at the intersection.

If a roundabout design is selected, it should be located as far south as possible to maximize the storage length for queuing northbound traffic at the Juan Tabo Boulevard/Central Avenue intersection so that the queue does not extend into the roundabout's circulating roadway. Due to projected traffic volumes, a two-lane circulating roadway with two-lane approaches would be necessary.



A potential roundabout design for the intersection of Southern Avenue and Juan Tabo Boulevard.

Figure 6.e: Roundabout Option

6.5.6 Pedestrian Street Crossing Improvements

This Plan recommends the following pedestrian street-crossing improvements:

- Traffic signal installation at the Central Avenue/Elizabeth Street intersection (see 6.3.2)
- Street crossing redesign at all signalized intersections using the draft Great Street Facilities Plan for guidance
- Street crossing redesign at arterial street intersections with bicycle trails, lanes, routes, and boulevards

Some arterial street crossing elements to consider are as follows:

Median refuges, raised intersections, traffic control devices (including new or upgraded traffic signals, directional handicap accessible curb ramps, pavement markings, traffic stripes, in-roadway crossing lights, flashing beacons, bicycle-sensitive signal actuation devices, pedestrian countdown signals, vehicle speed feedback signs, pedestrian activated signal upgrades), and sight distance improvements.



This mid-block example contains elements that can be modified for a street intersection.

Figure 6.f: Median Refuge and Striped Pedestrian Crossing

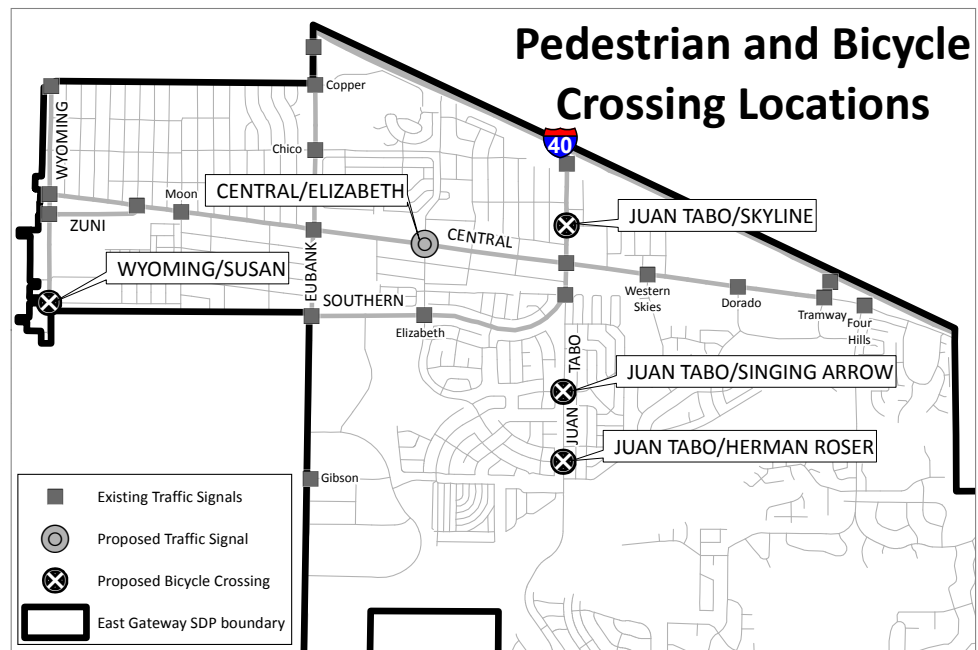


Figure 6.g: Pedestrian and Bicycle Crossing Locations

6.5.7 Traffic Calming

This Plan recommends street design modifications to control vehicle speeds and support bicycling and walking on neighborhood and arterial streets.

Traffic-calming measures either trick the eye or physically alter street width or alignment. The intent is to encourage drivers to slow down, focus their vision field, and become more aware of pedestrians, bicyclists, and other vehicles. Techniques generally fall into three categories: 1) physically narrowing the street or introducing visual elements to create a sense of enclosure or to break up views, 2) deflecting the vehicle path vertically, and 3) deflecting the vehicle path horizontally. Common techniques for arterial streets include narrowing the total roadway width, narrowing the travel lane width, and creating a sense of visual enclosure with medians and landscaped strips behind the curb.

Traffic calming plans should always address emergency vehicle response time and safety for cyclists. They have the greatest benefit when used throughout a district rather than in isolated locations. In fact, installation of a single device may divert traffic to neighboring streets, shifting the problem rather than resolving it.

Visual and Physical Techniques to Narrow a Street

Surface Striping, Change in Pavement Color, Landscaping, and Placing

Buildings close to the Street are techniques to visually narrow the roadway.

These techniques can make drivers more aware of surroundings and cause them to drive more slowly. Visual tools, particularly striping and landscaping, are often combined with physical changes to the roadway to maximize traffic calming.

Streets can be physically narrowed at intersections (curb extensions) and mid-block (chokers). Intersection narrowing helps to reduce pedestrian crossing times and distances and to meet Americans with Disabilities Act (ADA) requirements. Street medians can also be used to narrow street crossings by providing a refuge for cyclists and pedestrians as they negotiate one-way traffic on each 1/2 of the street width. These techniques are particularly useful near school zones, in areas with high elderly and disabled populations, at bicycle crossings, bus stops or other areas with pedestrian movement. Mid-block narrowing is used primarily to slow down traffic. Many narrowing techniques require landscaping to give motorists advance warning and to enhance the calming effect.

Medians or Center Islands slow traffic in three ways: visually tightening the road, slowing turn speeds, and creating narrow channels. When combined with crosswalks and divided to provide a crossing entirely at street level they are called Refuge Islands for pedestrians. The minimum preferred width for medians with a refuge is 8 feet. Medians represent one of the most affordable and least intrusive tools.

Curb Extensions (Bulb-outs) slow speeds at intersections and mid-block locations. They can be used mid-block to create chokers or chicanes and to provide discrete on-street parking areas that do not disrupt emergency vehicle access to critical streets. On streets with high vehicular traffic volume and a bicycle route, boulevard or lane, they must include a channel between the curb and extension.

Chokers (Neckdowns) reduce traffic speeds by narrowing passageways to a one-way entry or exit point, and can be very attractive when properly landscaped.

Vertical Deflection

Speed Humps are both inexpensive and effective (a 14' parabolic hump can slow traffic to about 22 mph), but can be noisy, devalue a neighborhood, and affect emergency response times.

Speed Tables are a special form of speed hump that feature flat tops. They are the best tool for pedestrian and bicyclist crossings, and are typically used on local streets. They are more suitable for collector streets than traditional speed humps, but should not be used where volumes exceed 10,000 vehicles per day, on bus

routes, or on prime emergency response routes. Speed tables can be placed mid-block (used as raised cross-walks) or at intersections.

Raised Intersections are flat, raised areas covering an entire intersection, with ramps on all approaches and often with brick or other textured materials on the flat section. They slow traffic in three ways: creating a distinct shape that draws a motorist's attention, creating a vertical deflection that forces a low-speed approach, and highlighting the area as a pedestrian space. Raised intersections can be used with narrow intersections in commercial areas, but are relatively costly.

Horizontal Deflection

Chicanes are used mid-block by incorporating curb extensions, striping, islands, or even on-street parking to divert traffic from a straight course. Also called "slow points," chicanes hold speeds to 15 to 20 miles per hour and may result in a volume reduction. On low volume streets, no treatments are needed for bicycles, but on higher volume avenues and bicycle boulevards, channels should be made for bicycles between the curb and the chicane.

Mini-roundabouts act as both traffic-calming devices and intersection control devices in place of four-way stops or traffic signals. They can be designed for arterial and collector streets or for local streets, particularly bicycle boulevards.

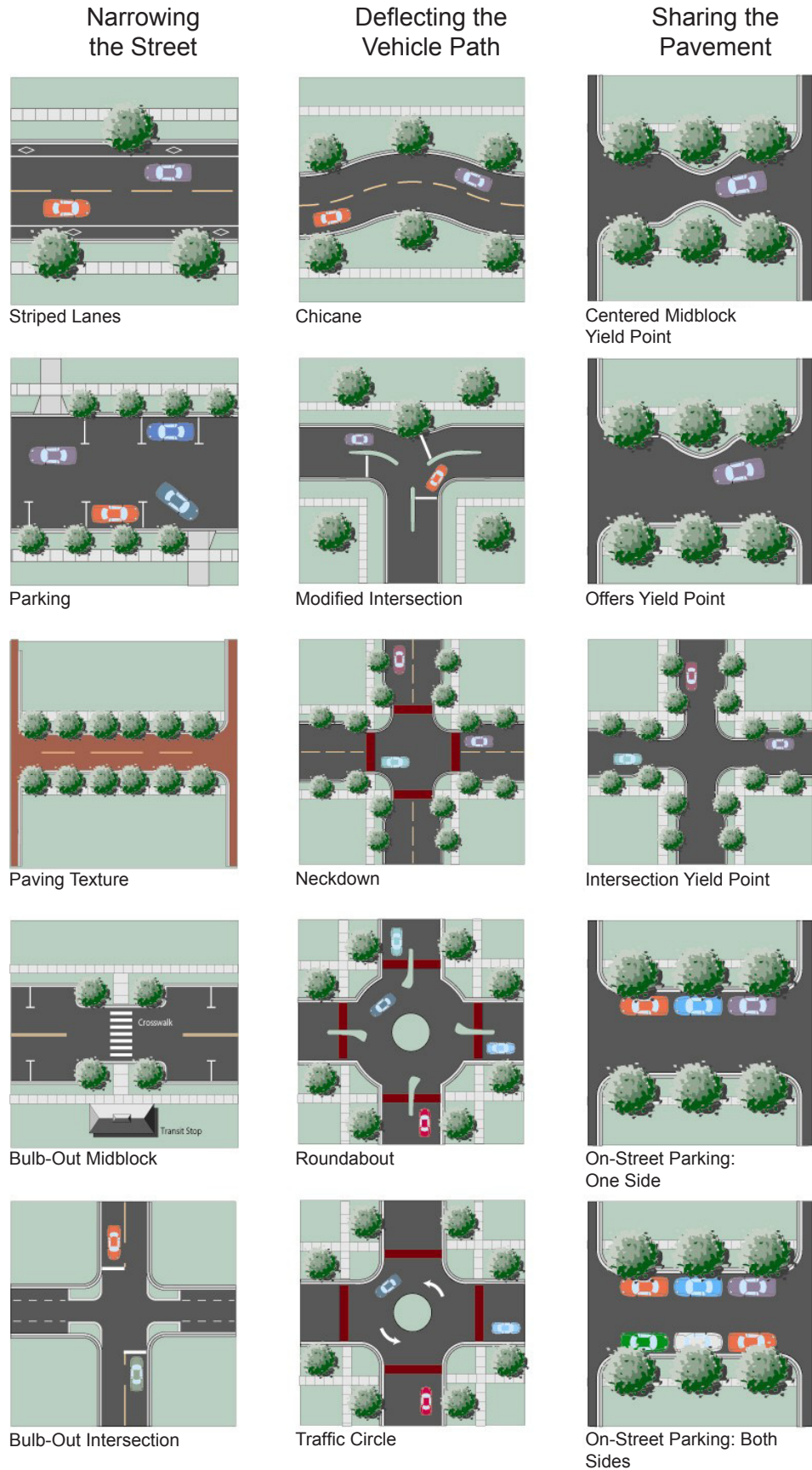
Project Recommendations

Planning participants indicated that Chico Road, Skyline Road, and Singing Arrow Avenue need techniques to control the high-speed traffic they convey through residential neighborhoods. These three streets and others noted in section 6.3.1 of this chapter need traffic calming techniques. Each street will require study to determine appropriate methods. The following are suggestions.

Chico Road and Skyline Road are both wide enough to accommodate on-street bicycle lanes. If bicycle lanes are added, speed tables can be fit between the bicycle lanes. If traffic calming is implemented before bicycle lanes, care should be taken to continue bicycle lane striping over the speed table surface to separate vehicle lane space from bicycle lane space. Wide bicycle lanes are used so that cyclists can avoid the slopes at the curbside ends of the speed table that are often used to allow curb-and-gutter drainage to function properly.

Singing Arrow Avenue is a residential street used as a cut-through street for drivers avoiding Central and Southern Avenues. It is an existing bicycle route, but this Plan recommends that it be designated and designed as a bicycle boulevard. Speed tables are a balanced approach for calming traffic while continuing to let bicycles move. Chicanes may also slow moving traffic to speeds that allow cyclists to comfortably share the lane with other vehicles.

Figure 6.h: Examples of Traffic Calming



7 The Plan – Parks and Major Public Open Space

7.1 Introduction

East Gateway's Public Parks and Major Public Open Space open space are essential to community health and well-being, providing spaces for exercise, recreation and visual relief within the urban fabric.

7.2 Plan Goal Addressed by Recommendations

Goal 5 – Provide Accessible Parks, Major Public Open Space and Community Programs that serve the entire East Gateway community.

This Plan recommends expansions and further development of existing parks to improve outdoor recreation opportunities for East Gateway residents.

This Plan recommends development of additional parks in areas that are underserved by park facilities.

This Plan recommends continued acquisition of Major Public Open Space to create a continuous Major Public Open Space corridor in the Tijeras Arroyo and access points and trails that lead to the arroyo and to Major Public Open Space in the Sandia and Manzano Mountains foothills.

This plan recommends habitat enhancement, restoration, protection, partnerships and legislative agreements to improve habitat in the Tijeras Arroyo as indicated in the *Resource Management Plan for Tijeras Arroyo Biological Zone*, City of Albuquerque, Parks and Recreation Department Open Space Division, September 2008. (See Appendix D.)

7.3 Recommendations

Parks

This Sector Development Plan recommends the following projects to provide a broader range of parks and recreational amenities in East Gateway:

- **Expand Sandia Vista Park:** The New Mexico Department of Transportation owns land just south of the I-40 Bicycle Trail adjacent to Sandia Vista Park. This parcel is appropriate for park expansion, including playfields and a better entrance to the I-40 Bicycle Trail to the north. The City should explore the feasibility of a land trade or purchase. Additional funding will be required for site improvements.
- **Expand Park Uses at Singing Arrow Park:** Protect, but develop the archeological site with natural vegetation, carefully placed trails, interpretive signage, and outdoor furniture.

- ***Continue to Develop Manzano Mesa Park:*** Continue ongoing development of playfields and recreational amenities at the park.
- ***Construct a Four Hills Subdivision Park:*** Acquire an approximately 2-acre site that includes a well from Four Hills Country Club. Develop the site as a neighborhood park.
- ***Develop an additional park adjacent to or near Central Avenue east of Eubank Boulevard:*** Determine the feasibility of acquiring state-owned land near the Central Avenue/Eubank Boulevard intersection. The State owns parcels north and south of Central Avenue. Both sites were leased to mobile home park owners. Area residents have suggested that one of the sites with many mature trees could be appropriate for a park. If these sites are not appropriate, secure another site to provide recreation and visual relief for local residents, employees, and shoppers.

Major Public Open Space

This plan recommends the following projects to improve the East Gateway Major Public Open Space experience for Albuquerque residents:

- ***Tijeras Arroyo Biological Zone (TABZ) Major Public Open Space (MPOS):*** Continue to acquire private land parcels within the arroyo to create a Major Public Open Space corridor that conserves natural resources, wildlife, environmental features and cultural resources; provides opportunities for education and low-impact recreation; and enhances vegetative biodiversity habitat.
- ***La Cañada Park:*** Develop La Cañada Park as Major Public Open Space, using a portion of the land to accommodate a small number of parking spaces, as well as a formal trailhead/access point to the Tijeras Arroyo. Acquire additional land if necessary. Minimize negative impacts on surrounding private property.
- ***Ravine Park:*** Transfer Ravine Park from the Public Parks inventory to the Major Public Open Space inventory. Develop Ravine Park as Major Public Open Space.
- ***Major Public Open Space property to the east of the East Gateway plan area:*** Continue City collaboration with the Trust for Public Lands and Bernalillo County to acquire lands to create access between the Manzano and Sandia Mountain Major Public Open Space lands.
- ***Access Points to Tijeras Arroyo:*** Formalize public access to the Tijeras Arroyo at Four Hills Road and Juan Tabo Boulevard after a continuous Major Public Open Space corridor is established. Evaluate the costs and safety issues of formalizing the steep, unmaintained trail from the top of the bluff at Singing Arrow Park.

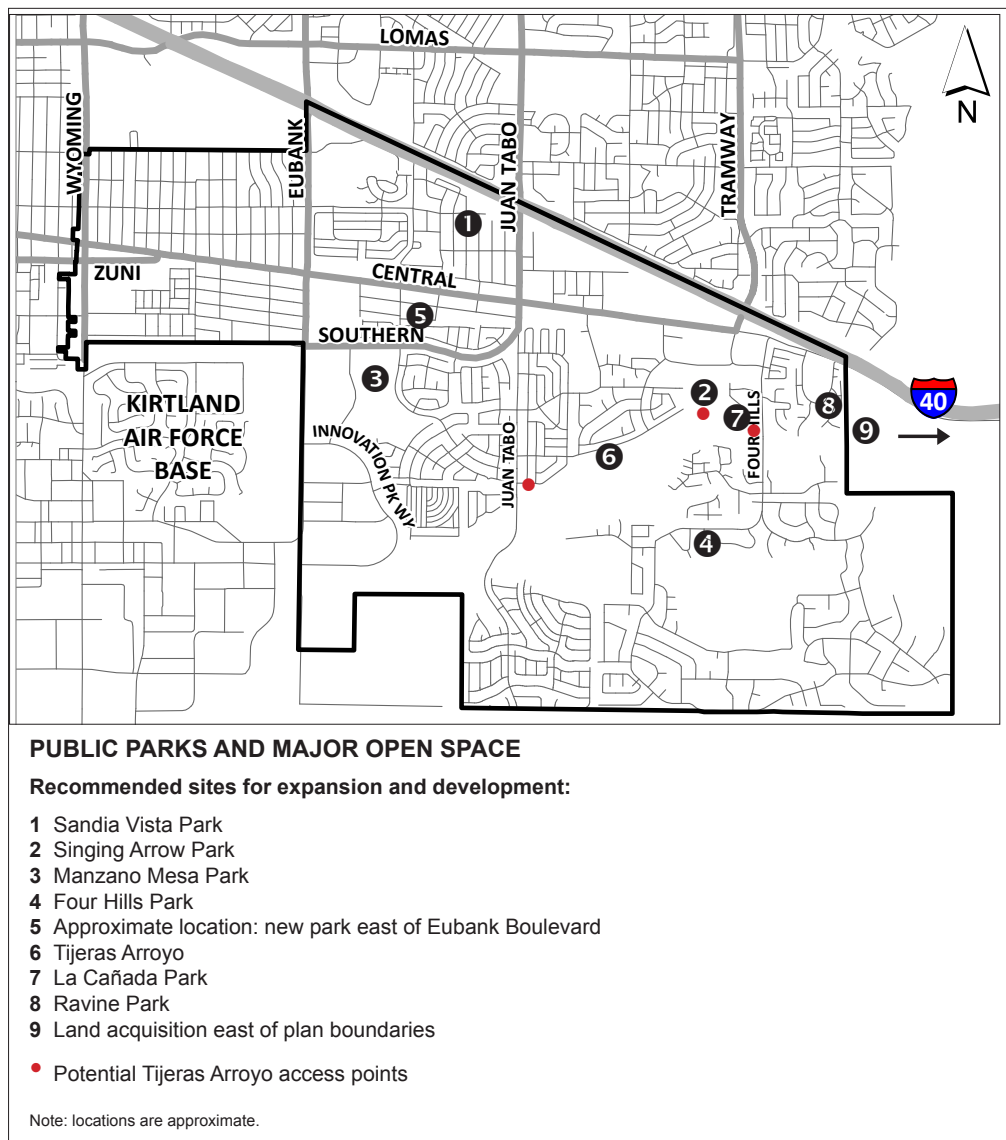


Figure 7.a: Map of Public Parks and Major Open Space

8 The Plan – Public Buildings, Street Lights and Alternative Energy Demonstration Projects

8.1 Introduction

The following recommendations are intended to improve the existing community center, provide local library services, ensure good emergency services response time, provide a safe level of street lighting, and explore the feasibility of alternative energy demonstration projects.

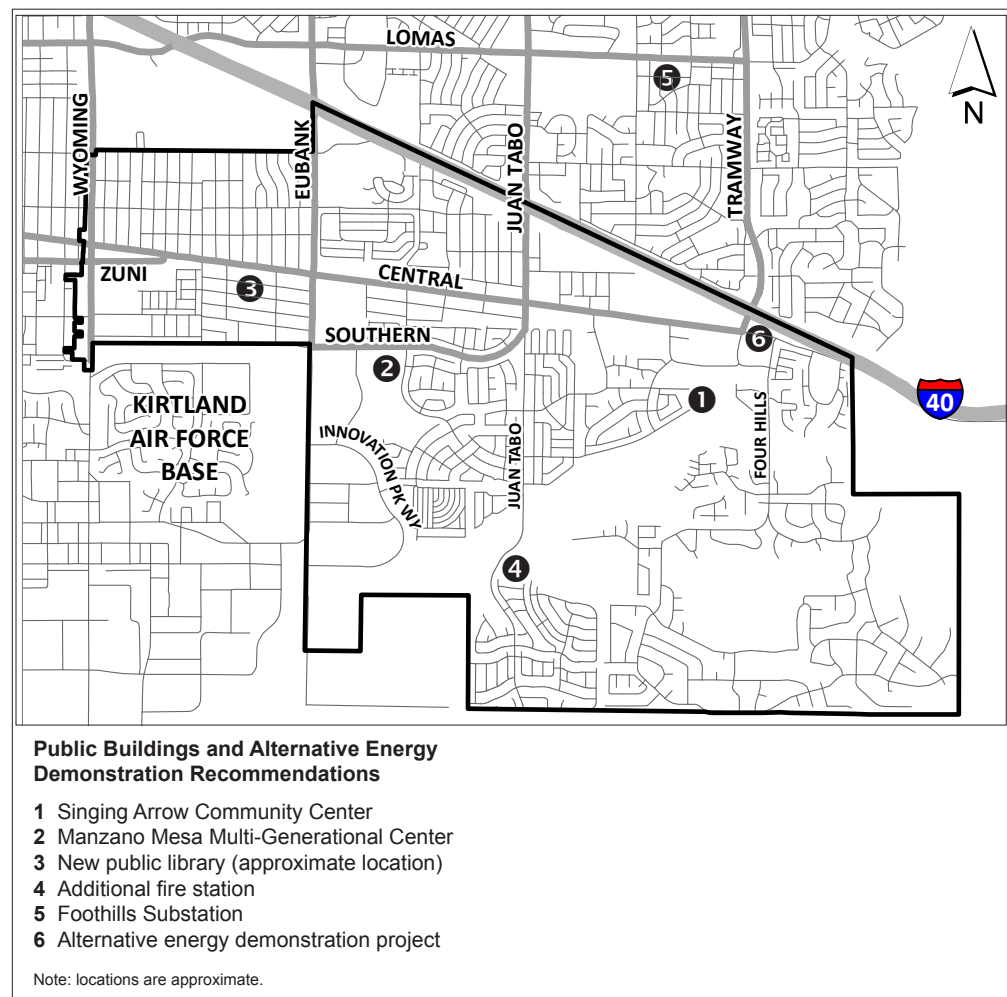


Figure 8.a: Map of Facility Recommendations

8.2 Plan Goals Addressed by Recommendations

Goal 2 – Create a safe, well-maintained, attractive community.

Goal 6 – Support existing stable thriving residential neighborhoods and transform others.

This Plan recommends expansion and/or possible long-term replacement of Singing Arrow Community Center to better serve neighborhood needs.

This Plan recommends construction of a full-service library to meet East Gateway needs, to be located at a high-traffic, visible location that is accessible by public transit.

This Plan recommends future construction of an additional fire station.

This Plan recommends a feasibility study to identify a visible location for an alternative energy demonstration project, such as small vertical axis wind turbines or a solar energy project.

This Plan recommends additional street lighting in key areas to improve neighborhood safety.

8.3 Recommendations

This Sector Development Plan recommends the following projects to address East Gateway needs.

Community Centers

- ***Singing Arrow Community Center:*** To function as a vibrant community hub, Singing Arrow Community Center must be enlarged. The Plan recommends a study of community needs to determine how they might be addressed at an expanded Singing Arrow facility. The study should consider: classes and programs for local children and adults, indoor and outdoor recreation, a commodities distribution program, and transportation services for children. The needs assessment should consider building expansion, modular building retention and long-term replacement of the existing structures.
- ***Manzano Mesa Multi-Generational Center:*** The Department of Senior Affairs doesn't anticipate any major changes to the facility, nor does it anticipate construction of any other senior-serving facilities in East Gateway. Expansion of the adjacent playing fields will continue, however, as part of the Parks and Recreation program.

Libraries

New Library: In its 2007 facilities master plan, the Library System identifies a new full-sized library for the East Central area as its second priority, after the construction of a West Central library. This new library should be located at a visible location on a busy street with good public transit.

Fire

Additional Fire Station: As part of the development agreement for the Juan Tabo Hills subdivision, 1.25 acres were donated to provide a site for a new fire station. No capital funding was available or programmed in 2009. An additional East Gateway station could provide more emergency medical services and fire protection coverage in the future. This potential station location and future construction will be studied and managed by the Albuquerque Fire Department.

Police

Substation Renovation: The Foothills Substation on Lomas Boulevard NE requires some renovations but the Albuquerque Police Department deems it otherwise adequate to serve the needs of the area. Renovations should be programmed through funded and scheduled facility maintenance cycles.

Utilities

Alternative Energy Demonstration Project: East Gateway is uniquely positioned to take advantage of the wind that channels through Tijeras Canyon. Some community residents have suggested that the New Mexico Department of Transportation land between La Luz de Amistad Park and I-40 be used for a wind or solar energy demonstration project. Other prominent locations should also be considered as part of a feasibility study for a project of this type.

New turbines are beautifully designed, quiet and designed to fit in an urban environment. This type of installation could enhance the East Gateway to Albuquerque, a demonstration of the City's commitment to innovation in green energy production.



Figure 8.b: Conceptual Design for Dongtan Eco-city, Shanghai, Arup, showing small-scale wind turbines

Streetlights

Additional Streetlights: Areas identified for attention in the near term are:

- ***Singing Arrow Neighborhood:*** Bond funding was available in 2009 and should be expended to ensure lighting improvements.
- ***Cochiti/Acoma area:*** A project was underway in 2009 to install four new streetlights.
- ***Skyline Road/Figueroa NE:*** Funding should be secured no later than FY 2010 to install streetlights needed for safety and security.

In addition, funding should be secured over two to three years after Plan adoption to improve street lighting in other locations. See maps on the following pages. Mid-block street lighting may require additional easements to connect with existing electric distribution lines located along rear lot lines.

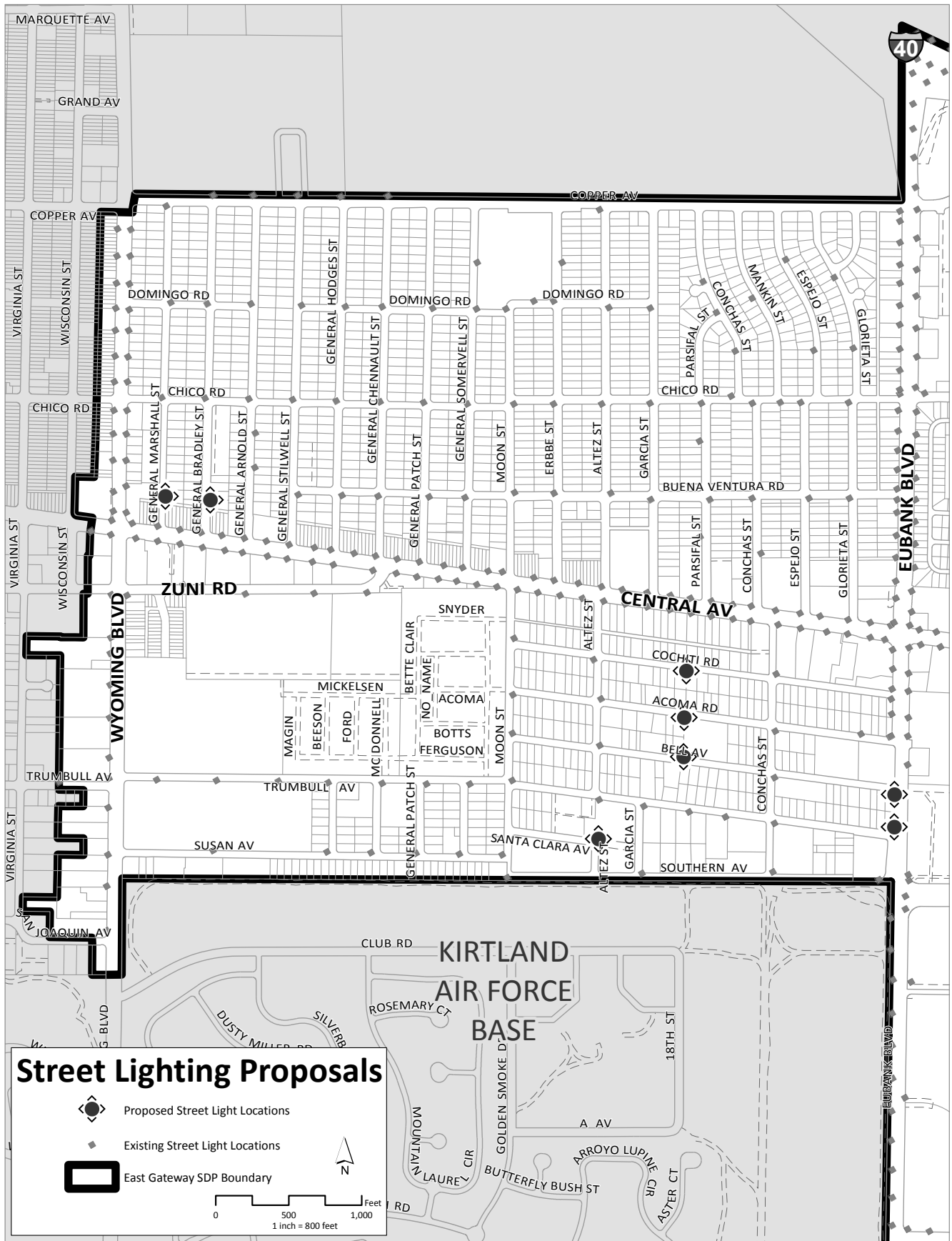
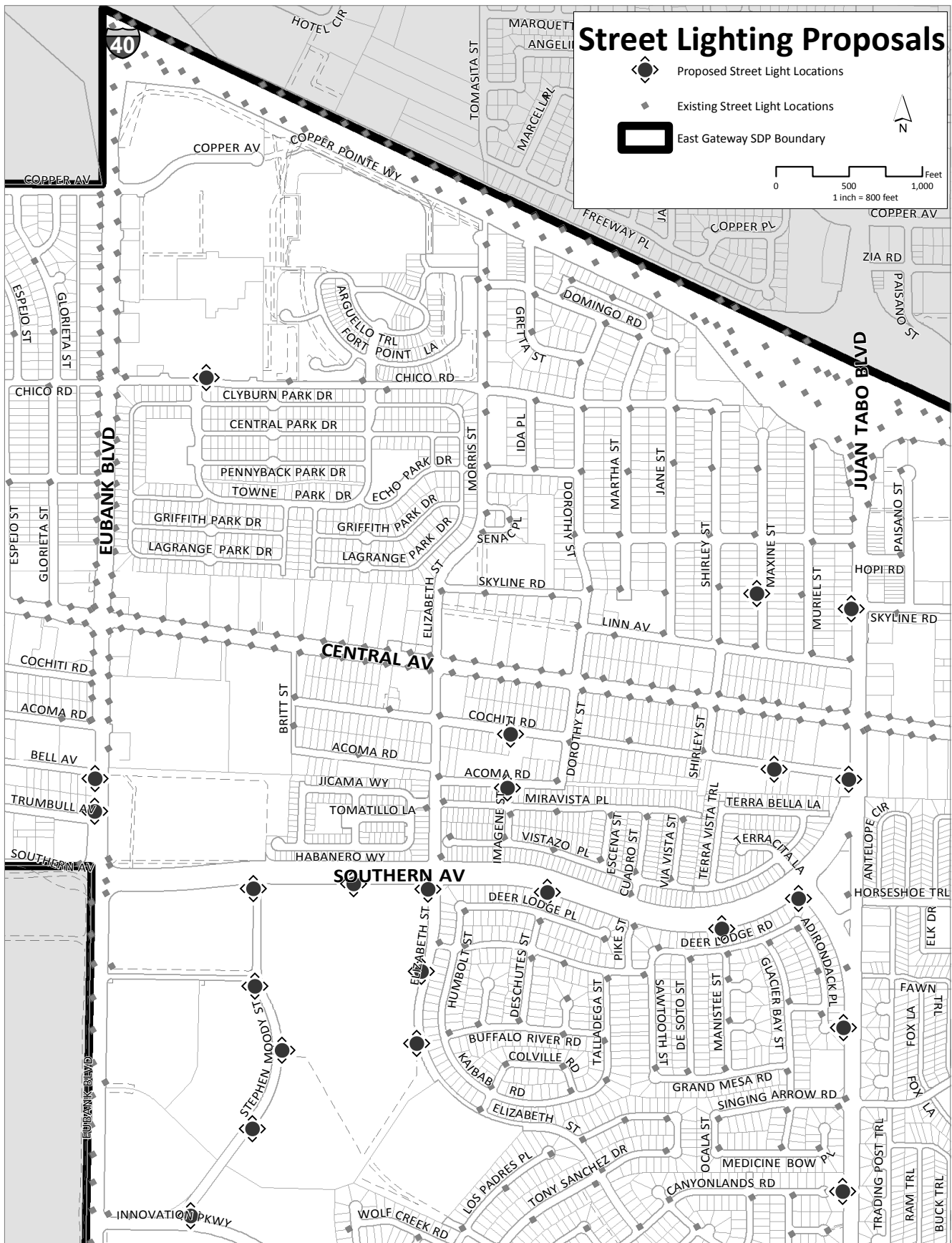
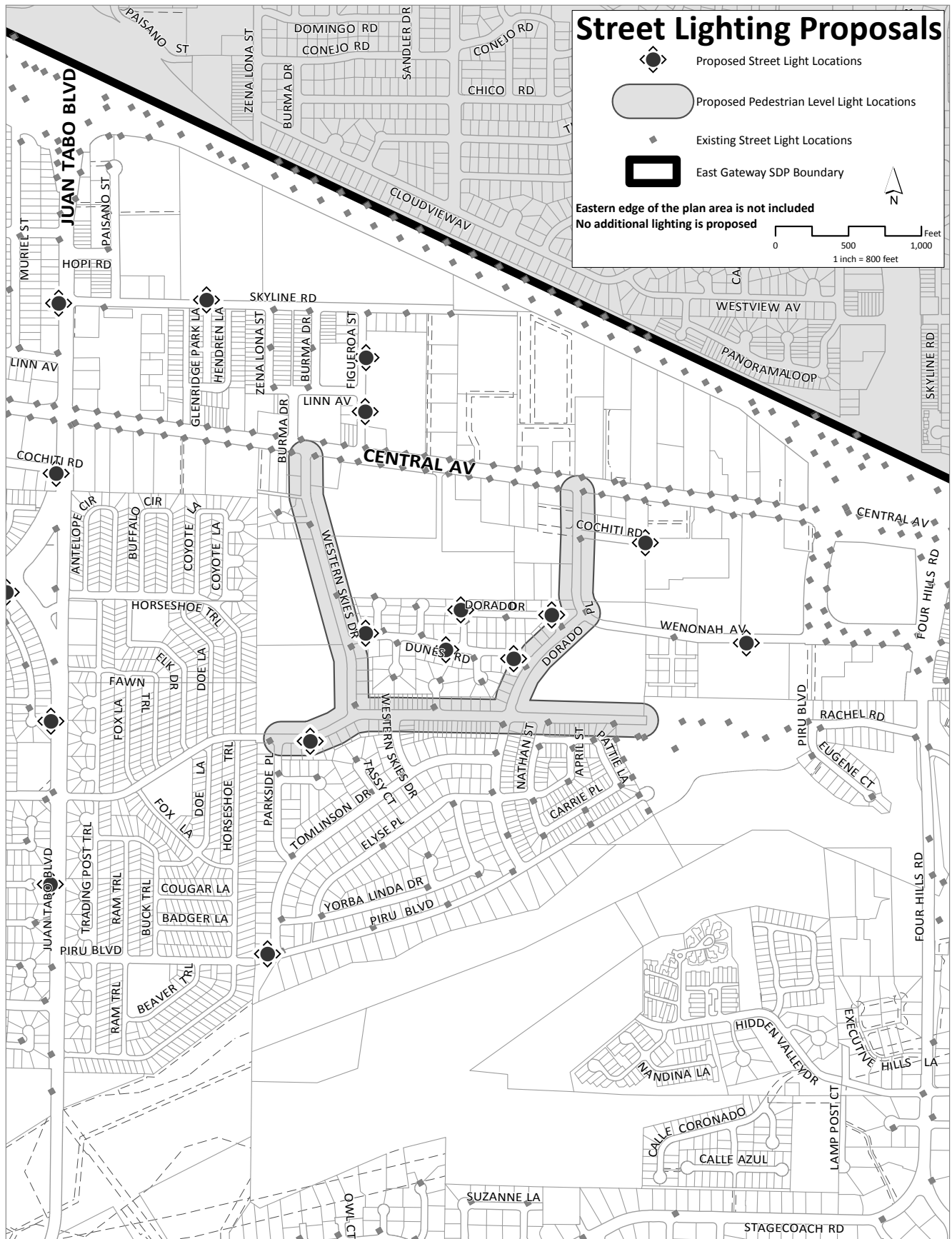


Figure 8.c: Maps of Street Lighting Proposals





9 The Plan – Programs and Policies

9.1 Introduction

This chapter recommends actions that will help fulfill East Gateway Sector Development Plan goals: 1) To create a safe, well-maintained and attractive community; 2) To enable the continued existence and development of thriving businesses to provide jobs and local services; 3) To design and build streets and trails that offer multiple efficient, safe transportation choices; 6) To support existing stable, thriving residential neighborhoods and transform others, and 7) To ensure well-maintained, safe housing for low-income households.

Included are recommendations to initiate proactive crime prevention programs and code enforcement in East Gateway, to reclassify Central Avenue as a major transit corridor, to continue actions to reclassify Southern Avenue as a collector street, and to limit additional vehicular access to Four Hills Road.

9.2 Recommendations



Safe, Attractive Residential Neighborhoods and Rental Housing

- **Crime Prevention:** This Plan recommends targeting East Gateway areas the Police Department has identified as high crime areas with substandard housing. The Plan recommends a combination of City police initiatives and non-profit agency programs that include the Crime-Free Multi-Housing Program, the Safe City Strike Force, Neighborhood Watch programs, Crime Prevention through Environmental Design (CPTED), and Albuquerque Family Advocacy Center programs.
- **Housing, Zoning and Building Code Enforcement:** This Plan recommends proactive investigation and enforcement of housing, zoning and building codes at least every two years to ensure safe rental housing. Include all East Gateway area public housing and all privately owned East Gateway rental housing.

The Plan also recommends that Albuquerque Housing Services assess and improve the landscaping and physical condition at the three East Gateway public housing projects.

Attractive Businesses

- **Zoning and Building Code Enforcement:** This Plan recommends proactive enforcement of zoning and building codes for all non-residential property to occur at least every two years.

Street Improvement

- **Corridor Reclassification:** This Plan recommends that the City of Albuquerque/Bernalillo County Comprehensive Plan's Figure 30 (a map of Development Areas with Activity Centers and Transportation Corridors) be amended to reclassify Central Avenue from Louisiana Boulevard to

Tramway Boulevard from an Enhanced Transit Corridor to a Major Transit Corridor. This action will be initiated by the City to acknowledge Rapid Ride service on this stretch of Central Avenue.

- **Street Reclassification:** This Plan recommends that the City Council resolution to reclassify Southern Avenue to a collector street without access control be taken to the Metropolitan Transportation Board for action.

This action will be initiated by the City and be done in cooperation with the Mid-Region Council of Governments. It acknowledges the changed conditions that eliminate the future development of Southern Avenue as a connecting segment of a loop road extending through Kirtland Air Force Base. With reclassification, Southern Avenue can be redesigned as an urban collector street to serve local needs.

Four Hills Village Road

- **Vehicular Access Control Policy:** The City shall not approve any roadway that expands the area currently served by Four Hills Road. This prohibition shall not limit emergency access to the Four Hills area or bicycle and pedestrian trail, lane or route access.

10 Plan Implementation

The following charts summarize the Plan's project, program and policy recommendations. Where possible they include project descriptions and locations, lead agency, estimated cost, possible funding, and implementation phasing. Policy proposals include a policy description and lead agency.

10.1 East Gateway Project and Policy Recommendations

Note: Feasibility and/or design studies will be required before cost estimates are developed.

All Land Use Regulation changes are contained in Chapter 5.

General Obligation Bonds = G.O. Bonds

Location in Plan	Project Name and Location	Agency(ies)	Cost (if known)	Funding Source(s)	Project Elements Underway	Timing (estimated)		
						0-4 Years	5-8 Years	9 + Years
	Transportation							
6.3.1	Designate and design bicycle boulevards to improve and complete portions of the bicycle network, to include Chico Road, Skyline Road, Moon Street, Morris Street, Herman Roser Avenue, Tony Sanchez Drive, Singing Arrow Avenue, and Gibson Boulevard.	Department of Municipal Development	See Note.	G.O. bonds, State Legislature, Council appropriations		x	x	
6.3.2	Install traffic signal on Central Avenue at intersection with Elizabeth Street Bicycle Lane.	Department of Municipal Development	See Note.	G.O. bonds, State Legislature, Council appropriations		x		
6.3.2	Evaluate arterial street crossings for each East Gateway bicycle route, boulevard, lane, and trail to determine the appropriate safe crossing features needed.	Department of Municipal Development	See Note.	G.O. bonds, State Legislature, Council appropriations		x	x	
6.3.3	Complete the bicycle lane on Eubank Boulevard from the I-40 overcrossing to Southern Avenue.	Department of Municipal Development	See Note.	G.O. bonds, State Legislature, Council appropriations			x	
6.3.4	Evaluate and improve trail entrances to improve safety. Plan and implement improved entrance visibility.	Department of Municipal Development	See Note.	G.O. bonds, State Legislature, Council appropriations		x		

Location in Plan	Project Name and Location	Agency(ies)	Cost (if known)	Funding Source(s)	Project Elements Underway	Timing (estimated)		
						0-4 Years	5-8 Years	9 + Years
6.3.4	Negotiate with the NMDOT to acquire land between Sandia Vista Park and the I-40 trail for park development, and to ensure better access to the I-40 Bicycle Trail.	Department of Municipal Development	See Note.	G.O. bonds, State Legislature, Council appropriations		x		
6.3.5	Improve bicycle trail maintenance, working with volunteers and City Parks and Recreation to accomplish regularly scheduled maintenance.	Department of Municipal Development	See Note.	City operational budget, grants		x	x	x
6.4	Identify, acquire and develop a visible and accessible site for a park and ride lot and Rapid Ride transfer center on or near east Central Avenue between Western Skies Drive and Tramway Boulevard.	ABQ Ride	See Note.	G.O. bonds, State Legislature, Council appropriations		x		
6.5.1	Redesign Central Avenue/ Eubank Boulevard intersection to minimize intersection delay.	Department of Municipal Development	Funded	G.O. bonds, State Legislature, Council appropriations	Construction scheduled to begin late 2010	x		
6.5.1	Study, design and build improvements at the Central Avenue/Juan Tabo Boulevard Intersection in conjunction with design for Southern Avenue and Juan Tabo intersection.	Department of Municipal Development	See Note.	G.O. bonds, State Legislature, Council appropriations	Planned for next several years	x	x	
6.5.2	Initiate a spot median project on Central Avenue between Eubank and Wyoming Boulevards. Ensure that the appearance of the turning lane between medians is designed to differentiate it from through-traffic lanes, using a different-colored finishing material.	Department of Municipal Development	See Note.	G.O. bonds, State Legislature, Council appropriations		x	x	
6.5.3	Redesign and landscape Wyoming Boulevard medians to improve safety and aesthetics.	Department of Municipal Development	See Note.	G.O. bonds, State Legislature, Council appropriations		x	x	

Location in Plan	Project Name and Location	Agency(ies)	Cost (if known)	Funding Source(s)	Project Elements Underway	Timing (estimated)		
						0-4 Years	5-8 Years	9 + Years
6.5.4	Redesign and reconstruct Southern Avenue between Eubank and Juan Tabo Boulevards to reflect changes resulting from reclassification as an urban collector street and removal of limited access status.	Department of Municipal Development	See Note.	G.O. bonds, State Legislature, Council appropriations		x	x	
6.5.5	Study the feasibility of a roundabout or alternative intersection design at the junction of Southern Avenue and Juan Tabo Boulevard to assist traffic flow and decrease delays.	Department of Municipal Development	See Note.	G.O. bonds, State Legislature, Council appropriations		x		
6.5.6	Improve safety of pedestrian street-crossings through the following measures: traffic signal installation at Central Avenue and Elizabeth Street intersection (see also 6.3.2), crossing redesign at all signalized intersections and crossing redesign at arterial street intersections with bicycle trails, lanes, routes, and boulevards.	Department of Municipal Development	See Note.	G.O. bonds, State Legislature, Council appropriations		x	x	x
6.5.7	Design and construct street modifications to control vehicle speeds and support bicycling and walking on neighborhood and arterial streets.	Department of Municipal Development	See Note.	G.O. bonds, State Legislature, Council appropriations		x	x	x
	Parks							
7.3	Work with NMDOT to acquire property adjacent to Sandia Vista Park for park playfields and other park amenities. (See also 6.3.4.)	Parks and Recreation	See Note.	Trade, G.O. bonds		x	x	
7.3	Examine feasibility of low-impact use of archeological site at Singing Arrow Park, e.g., natural vegetation, pathways, outdoor furniture.	Parks and Recreation	See Note.	G.O. bonds, Council appropriation		x	x	
7.3	Continue development of Manzano Mesa Park to meet community needs for recreation facilities.	Parks and Recreation	See Note.	G.O. bonds		x	x	

						Timing (estimated)		
Location in Plan	Project Name and Location	Agency(ies)	Cost (if known)	Funding Source(s)	Project Elements Underway	0-4 Years	5-8 Years	9 + Years
7.3	Acquire two-acre parcel of land from Four Hills Country Club for use as a neighborhood park and well site.	Parks and Recreation	See Note.	G.O. bonds, Council appropriation		x	x	
7.3	Develop an additional park adjacent to or near Central Avenue. Among other options, assess feasibility of acquiring and developing State-owned land parcels near Central Avenue and Eubank Boulevard.	Parks and Recreation	See Note.	Trade, G.O. bonds, Council appropriation		x	x	
	Major Public Open Space							
7.3	Continue to acquire private lands within Tijeras Arroyo to consolidate public ownership, provide opportunities for education and low-impact recreation, and improve and enhance habitat.	Open Space Division	See Note.	G.O. bond, Council appropriation		x	x	x
7.3	Develop La Cañada Park as Major Public Open Space. Develop a small parking lot and a trailhead/access point.	Open Space Division	See Note.	G.O. bond, Council appropriation			x	
7.3	Transfer Ravine Park from the Parks inventory to the Major Public Open Space inventory for appropriate development as open space.	Open Space Division	Administrative action			x		
7.3	Continue collaboration with the Trust for Public Lands to acquire Major Public Open Space parcel directly east of East Gateway as a means to create a "gateway."	Open Space Division	Administrative action, various funding sources	G.O. bond, Council appropriation, legislative appropriation	The City has appropriated \$400,000.	x	x	
7.3	Enable pedestrian connections between the Sandia and Manzano Mountains and complete public ownership of Tijeras Arroyo through Tijeras Canyon.	Open Space Division	See Note.	G.O. bond, Council appropriation, legislative appropriation		x	x	

Location in Plan	Project Name and Location	Agency(ies)	Cost (if known)	Funding Source(s)	Project Elements Underway	Timing (estimated)		
						0-4 Years	5-8 Years	9 + Years
7.3	Formalize access to Tijeras Arroyo at Juan Tabo Boulevard and Four Hills Road. Evaluate costs and safety issues of formalizing the steep, unmaintained trail from the top of the bluff at Signing Arrow Park.	Open Space Division	See Note.	G.O. bond, Council appropriation, legislative appropriation		x	x	
	Community Centers							
8.3	Prepare a feasibility study to expand or replace Singing Arrow Community Center to address unmet needs for programs, indoor recreation, medical services, and other programs.	Family and Community Services	See Note.	G.O. bond, Council appropriation, legislative appropriation		x		
	Library							
8.3	Acquire land, plan and construct a full service library in a visible and accessible location within East Gateway to address a library service gap.	Albuquerque/Bernalillo County Library System	Approximately \$9 million plus land acquisition and materials purchases	G.O. bond, Council appropriation, legislative appropriation		x	x	
	Fire and Emergency Services							
8.3	Construct an additional fire station to improve East Gateway service coverage.	Albuquerque Fire Department	See Note.	G.O. bond, Council appropriation, legislative appropriation			x	

Location in Plan	Project Name and Location	Agency(ies)	Cost (if known)	Funding Source(s)	Project Elements Underway	Timing (estimated)		
						0-4 Years	5-8 Years	9 + Years
	Police							
8.3	Schedule needed refurbishment to the Foothills Substation as part of ongoing facility maintenance.	Albuquerque Police Department	See Note.	Maintenance funding		x	x	x
8.3	Use the existing multi-agency process to coordinate "strike force" approaches to address crime, housing conditions and other community safety and appearance issues.	Albuquerque Police Department, other agencies	Operational funding	Operational funding		x	x	x
	Street Lighting							
8.3	Install additional street lights in the Singing Arrow neighborhood to improve safety and visibility.	Department of Municipal Development	\$87,500	Funding secured	Installation anticipated for late 2009	x		
8.3	Install additional streetlights in the Cochiti/Acoma SE area to improve safety.	Department of Municipal Development	See Note.	Funding secured	2010	x		
8.3	Install streetlights for safety and crime prevention in the area near Figueroa and Skyline NE.	Department of Municipal Development	See Note.	G.O. bonds, Council appropriations		x		
	Alternative Energy							
8.3	Examine the feasibility of an alternative energy demonstration project for East Gateway that focuses on either wind or solar energy for electric generation, and also provides a unique gateway project to showcase Albuquerque's commitment to green energy.	Department of Municipal Development	See Note.	G.O. bonds, grants		x	x	

Location in Plan	Project Name and Location	Agency(ies)	Cost (if known)	Funding Source(s)	Project Elements Underway	Timing (estimated)		
						0-4 Years	5-8 Years	9 + Years
	Program and Policy Changes							
9.2	Target high crime areas that have substandard housing for initiatives that could include the following programs and others: Crime-free Multi-housing Program, Crime Prevention through Environmental Design and Albuquerque Family Advocacy Center programs.	Albuquerque Police Department, other agencies		Administrative, programmatic funding, grants		x	x	x
9.2	Ensure safe rental housing through consistent proactive investigation and enforcement of housing, zoning and building codes.	Albuquerque Police Department, other agencies		Administrative, programmatic funding		x	x	x
9.2	Assess, improve and consistently maintain the landscaping and physical conditions at three East Gateway public housing projects.	Albuquerque Housing Services		Maintenance and operations funding		x	x	x
9.2	Enforce zoning and building codes for all non-residential property, to occur at least every two years.	Planning Department, Albuquerque Police Department		Administrative, programmatic funding		x	x	x
9.2	Amend the Comprehensive Plan to reclassify Central Avenue between Louisiana and Tramway Boulevards as a Major Transportation Corridor.	Planning Department, Council approval				x		
9.2	Take the City Council resolution to reclassify Southern Avenue as a collector street to the Metropolitan Transportation Board for action.	City Council action / DMD				x		
9.2	Rescind Singing Arrow Neighborhood Plan upon adoption of East Gateway Sector Development Plan.	Planning Department, Council approval				x		

						Timing (estimated)		
Location in Plan	Project Name and Location	Agency(ies)	Cost (if known)	Funding Source(s)	Project Elements Underway	0-4 Years	5-8 Years	9 + Years
9.2	Do not approve additional street that would place additional traffic on Four Hills Road	Planning Department				x	x	x